



TRANSPORTATION ADVISORY COMMITTEE (TrAC)

AGENDA

Wednesday, May 22, 2019

5:15pm

Dinner (Committee/Staff)

Goodpasture Room

5:45pm - 7:35pm

Public Meeting Session

3050 North Delta Hwy, Eugene, OR 97408

- I. Introductions / Agenda Review – *Chair, Kevin Woodworth, 5 min.*
- II. General Public Comment, **5 min.**
- III. Approval of March 27, 2019 Minutes – *TrAC, 5 min.*
- IV. Capital Improvement Program (CIP) Next Steps – *All, 30 min.*
Staff will review the project list compiled by the TrAC, discuss the initial vetting, provide details on TSP projects identified that are on emergency routes and safety corridors, and review funding options.
- V. Local Access Roads (LARs) Policy Discussion – *All, 30 min.*
After a brief overview of the existing concerns around LARs, the TrAC will discuss potential policy changes for existing and future LARs.
- VI. Territorial Highway Project Update – *Becky Taylor and Kerry Werner, 20 min.*
Staff will provide a project overview in preparation for a public hearing at the TrAC July 24 meeting.
- VII. Info Share / Next Steps, – *All, 15 min.*
 - Legislative update
 - Traffic calming program

Attachments:

- March 27, 2019 TrAC meeting minutes (page 2-5)
- TrAC project brainstorm list for CIP prioritization (page 6-9)
- List of TSP projects on Emergency Routes and Safety Corridors (page 10)
- Local Access Roads – past BCC meeting materials (page 11-16)
- Local Access Roads – list of questions to explore for policy changes (page 17-18)
- Territorial Highway Reconstruction Project memo and materials (page 19-112)
- Traffic Calming Program memo (page 113-114)
- TrAC 12 Month Calendar (page 115)



TRANSPORTATION ADVISORY COMMITTEE (TrAC)

MINUTES

Wednesday, March 27, 2019

I. Introductions / Agenda Review

Chair Woodworth called the meeting to order at 5:45 p.m. Two new members were introduced: Don Saxon, West Lane Appointee, and John Marshall, At-Large Appointee.

Members Present: *Chair* Kevin Woodworth, At-Large Appointee; *Vice Chair* Gwen Jaspers, South Eugene Appointee; Don Saxon, West Lane Appointee; John Marshall, At-Large Appointee; and Robin Mayall, Springfield Appointee

Staff Present: Peggy Keppler, County Engineer; Kerry Werner, Design Engineer; Sasha Vartanian, Transportation Planning Supervisor; and Becky Taylor, Senior Transportation Planner

II. General Public Comment

None

III. Approval of February 20, 2019 Minutes

Vice Chair Jaspers moved approval of the minutes which was seconded by Ms. Mayall and unanimously approved.

IV. Road Maintenance Storm Response

Staff played a video report of Road Maintenance's response to the 2019 snow storm event. The video reported that crews worked over 4700 hours in the first week, compared to 2100 hours during the 2016 ice storm that had been declared a federal emergency by the Federal Emergency Management Agency (FEMA). From the coast to the Cascades, the crews cleared 265 roads of snow and debris and responded to more than 800 reported downed trees and branches.

The TrAC expressed appreciation for the Maintenance work and inquired about debriefing the event to determine areas that still needed attention. Ms. Keppler responded that Patence Winningham-Melcher, Lane County Emergency Management Manager, was working with Orin Schumacher, Road Maintenance Manager. The TrAC requested a presentation by Ms. Winningham-Melcher and Mr. Schumacher of their findings and

recommendations to help the TrAC understand where additional investments may be needed for preventative maintenance.

V. Capital Improvement Program (CIP)

The TrAC identified the following priorities for staff to consider for the upcoming CIP:

Project	Woodworth	Jaspers	Mayall	Marshall	Saxon
Marcola Rumble Strip Noise Mitigation	1				
Maintaining "Good" Countywide Pavement Condition	2				
Sustaining Emergency Routes	3	1			
Territorial/126 Intersection (TSP Project #144d)		2			
Territorial (Crow – Cottage Grove-Lorane)			1		
Active Transportation Plan			2		
Hwy 99 (Peebles – Creswell, TSP Project #69)			3		
Fox Hollow (near Raptor Center) – <i>shoulder widening on blind curves</i>			4		
Beaver-Wilkes Path (TSP Project #18a)			5		
Yolanda (TSP Project #155)			6		
Coburg Interchange (TSP Project #27)				1	
Coburg Road North (TSP Project #28)				2	
Coburg Freight Connection (TSP #29)				3	
Green Hill Road (TSP Project #56a)				4	
North Fork Siuslaw Road					1
Canary Road – <i>pull out areas</i>					2

Chair Woodworth said his priorities were reflective of the public testimony the TrAC received on Marcola, the importance of preventative maintenance, and the consequences of the snow storm. Vice Chair Jaspers echoed the priority of investing in emergency routes and specifically called out the Territorial/Hwy 126 intersection project as a potential emergency access need.

Ms. Keppler confirmed that Public Works would be making corrective actions on Marcola rumble strips this summer. She also noted that Lane County assumed jurisdiction of Territorial Highway on March 21, 2019. She explained how the funding associated with the transfer from the State will enable Lane County to reconstruct Territorial consistent with the corridor plan, from Gillespie Corners to the town of Lorane. Ms. Vartarian noted the opportunity for the TrAC to write a letter of support for a potential grant application.

Ms. Mayall identified priorities from the perspective of a cyclist. She clarified that the segment of Territorial Highway that was the most unsafe for cyclists was between Hamm Road and Gillespie Corners, noting this is where Jane Higdon was killed and that the highway segment should be renamed after her. She said a popular bicycle route was from Crow to Lorane via Territorial Highway and Hamm Road and how most of that route was safe for cycling, but that the chip seal on Lorane is a rough ride for cyclists. She noted that the 2018 overlay on Fox Hollow improved cycling conditions, but that more work was needed near the Raptor Center. Mr. Werner explained work planned for summer 2019 on Fox Hollow near the Raptor Center.

Mr. Marshall explained his priorities as a resident of the City of Coburg and from a freight perspective. He noted that Coburg expanded its urban growth boundary for industrial development east of I-5 that would benefit from the proposed interchange. He explained how his other priorities were to facilitate freight. He noted that trucks rely more on North Coburg Road and Green Hill Road to avoid Beltline.

Mr. Saxon explained his priorities around North Fork Siuslaw Road as being specific to its intersection with Hwy 126, with safe sight distance being blocked by the pillars on the bridge and trees on the south side of the highway. He said Canary Road needed two or three pull out areas especially near Ada Grange.

Ms. Vartanian concluded the discussion saying that staff would scope the feasibility of the TrAC recommendations and would present those findings at the next TrAC meeting.

VI. Info Share

Legislative Update: Ms. Vartanian shared that Senate Bill 443, requiring headlights in inclement weather, and Senate Bill 558, providing more local control on setting speed limits, require hearings by April 9th to continue. Interested parties are encouraged to reach out to Senator Lee Beyer.

Safety Update: Ms. Taylor noted that the next public meeting on Sears Road Fixed Object Removal project is on April 8, 6:00-8:00 p.m., at the Cottage Grove Community Center, to reconsider the recommendation previously reviewed by the TrAC as directed by the Board. She also explained safety improvements being pursued on Beaver-Hunsaker following the pedestrian fatality in January to include education and engineering actions. Education actions include pedestrian safety education and providing donated reflective vests to assisted care facilities in the area, and slow-down/neighbors-ahead signage in front yards along the corridor. Engineering actions include pavement markings and delineation to create walking space on Hunsaker, a pedestrian-activated flashing beacon, and a path along Beaver on Delta property; these actions are pending additional funding which is likely to be awarded by the Central Lane Metropolitan Planning Organization.

TrAC Recruitment: Ms. Vartanian shared that the remaining vacant position in Commissioner Buch's district was still in the recruitment process.

Lane ACT Report: Vice Chair Jaspers shared that the LaneACT supported all of the transit funding applications proposed at the March 13 meeting. She noting that there appeared to be sufficient statewide funding for all of the applications, and that instead of prioritizing the applications, the LaneACT would be providing additional supportive comments on the local applications for maintaining the Oakridge service and for a new pilot service between Eugene and Florence.

Local Access Roads: Ms. Keppler explained how the Board requested the TrAC provide direction on the County's Local Access Road (LAR) policy. She explained the difference between LAR and County roads, with the former not maintained by Lane County or eligible for County gas tax expenditures. She clarified that the road fund revenue is gas tax and not property tax, in response to a question about whether people on LARs pay lower property taxes. She noted that Oregon Revised Statutes contains procedures on public involvement regarding the disposition of LARs, but that additional procedures for transferring LARs to County roads needed to be developed. The TrAC is scheduled to discuss this item at their next meeting.

Chair Woodworth adjourned the meeting at 7:30 p.m.

Lane County Transportation Advisory Committee: Projects for Capital Improvement Program

Project		Comments		Response to TrAC	Current Project Cost	Proposed Project Cost
Road/Project Name	Description / TSP Project	TrAC	Staff			
Marcola Road	Rumble Strip Noise Mitigation	Need to respond to public testimony	Repairs to the inside of three curves will be made summer 2019.	Funding has already been set aside for Summer 2019 to modify the Marcola Road rumble strips in order to mitigate the noise issues.		
Countywide Pavement Condition	Maintaining "Good"	Want to avoid more costly repairs in the future				
Emergency Routes	Sustaining and Upgrading	Need to make sure emergency routes are available during disasters				
Territorial/126 Intersection	TSP Project #144d	This is on an emergency route and the additional lanes may help in an emergency.		Territorial/126 Intersection is under ODOT jurisdiction. The original intent of this project was to widen and add additional lanes to the intersection to reduce the wait time for vehicles. Future conversations with City of Veneta have highlighted the importance of this intersection as a gateway to the City. It is also an important intersection for people crossing the street. There is a desire to create a safer intersection for kids walking to the schools in Elmira. Increasing the lanes would widen the crossing distance for pedestrians and may create safety problems especially for the school children.	\$750,000	

Territorial Hwy (Gillespie Corners-Lorane Rd)	TSP Project #141	Gillespie Corners to Hamm Road is the most dangerous section for cyclists	Gillespie Corners to town of Lorane will be reconstructed with shoulder widening 2020-2023		141a - \$1,000,000 141b - \$7,000,000 141c - \$10,000,000	
Active Transportation Plan	TSP Project #7	Need to identify roads with most cycling demand and prioritize improvements, including smoother pavement than chipseals			\$300,000	\$150,000
Hwy 99 (Peebles – Creswell)	TSP Project #69	Heavily used by cyclists		Hwy 99 (Peebles-Creswell) is under ODOT jurisdiction. Staff is following up with City of Creswell to understand if this project is a priority for the City.	\$7,000,000	
Fox Hollow (near Raptor Center)	Shoulder widening on blind curves	The 2018 overlay south of the Raptor Center is in good shape for cycling, but north of the Raptor Center is a rough ride for cyclists, especially at blind curves	2019 slide repair near Raptor Center will include pavement overlay	Fox Hollow is under City of Eugene Jurisdiction		
Beaver-Wilkes Path	TSP Project #18a			The Beaver-Wilkes Path project is a longer-term project than the 5-year CIP. The project will happen just not in this CIP. The County hopes to get funding for the Beaver frontage portion of the path for construction by 2020.	\$2,700,000	
Yolanda Avenue	TSP Project #155		MPO funding awarded (\$1,581,186.17) for construction 2020		\$1,100,000	
Coburg Interchange	TSP Project #27	To support new development of industrial land included in UGB expansion east of I-5		Coburg Interchange is under ODOT jurisdiction.	\$35,000,000	

Coburg Road North	TSP Project #30	To support freight with more truck traffic to/from Junction City		A pavement overlay was just completed for Coburg North. New pavement usually lasts anywhere between 7-10 years. A new project will not be pursued before new pavement is needed again. If providing shoulders on each side is still a priority, we can push this project to the 2026 CIP plan.	\$7,900,000	
Coburg Freight Connection	TSP Project #29			Although construction of a new road for the east-west connection between Coburg Road and I-5 could be beneficial for freight traffic, the project is premature for the CIP at this time. The study of an East-West connection would need to determine feasibility and whether an exception to the Transportation Planning Rule (TPR) is required. Given the limited allowances provided in the TPR for this type of project, it is assumed an exception will be required. An exception cannot be given without first demonstrating there isn't capacity on existing facilities, including considering capacity improvements on those existing facilities. There are TSP projects that have yet to be built specifically to accommodate freight on existing facilities in the North Coburg Road area.	\$250,000	
Green Hill Road	TSP Project #56	To support freight which is using this road more to avoid Beltline; this road is also heavily used by cyclists			56a - \$500,000 56b - \$2,875,000 56c - \$600,000	

North Fork Siuslaw Rd/126 Intersection	Sight distance	The bridge pillars and trees along OR 126 are blocking views from N. Fork at the intersection		North Fork Siuslaw Rd/126 Intersection is under ODOT jurisdiction. Staff reached out to ODOT maintenance staff to share concerns about the trees blocking the sightline at the intersection. ODOT said the regularly trim the trees to keep the sightline as clear as possible.		
Canary Road	Pull-out areas			Staff investigated the need for vehicle turnouts on Canary Road. The purpose of vehicle turnouts is for faster vehicles to safely pass slower vehicles. Vehicle turnouts are mostly needed on Freight Routes and on steep roads. According to the Lane County Transportation System Plan, Canary Road is not a Freight Route; therefore, the road will not have many slow moving trucks. Canary Road is also not that steep. Crash data from the last 5 years was evaluated for Canary Road. There have been a total of 7 crashes, none of which were fatal. All but one of the crashes were at intersections and the one that wasn't was due to Ice. These crashes also occurred in the section of road where opportunities for slower moving vehicles to pull over appear plentiful. Canary Road has driveways that are approximately 1,000 feet apart. Staff recommend not including this project in the CIP based on the evaluation performed.		

Updated Costs

Project #	Project Name	Project Summary	Agency Partners	Construction Cost	Environmental Cost	Right-of-Way Cost	Estimated Design Cost	Administration Cost	Planning Cost	Estimated Cost	Total Cost with Inflation	Total Cost with Contingency
28	Coburg Road from Coburg Road North to Linn County line	Construct to major collector standards with two 11' travel lanes and 6' shoulders on both sides. Integrate system safety measures.	County	\$3,381,000	\$0	\$172,000	\$1,065,900	\$392,000	\$213,180	\$5,224,080	\$5,735,746	\$8,030,045
90a	Jasper-Lowell Road from Parkway Road to Pengra Road	Construct to freight route standards with 12' travel lanes and 6' shoulders on both sides.	County	\$2,397,750	\$0	\$139,000	\$761,025	\$278,000	\$152,205	\$3,727,980	\$4,093,113	\$5,730,358
100	Marcola Road from Wendling Road to Johnson Road	Construct to major collector standards with two 11' travel lanes and 6' shoulders on both sides. Integrate system safety measures.	County	\$2,984,250	\$0	\$173,000	\$947,175	\$346,000	\$189,435	\$4,639,860	\$5,094,306	\$7,132,028
101	Marcola Road/Brush Creek Road from Highway 228 to Camp Creek Road	Construct safety improvements including shoulder rumble strips and fixed object removal.	County	\$672,750	\$0	\$39,000	\$213,525	\$78,000	\$42,705	\$1,045,980	\$1,148,427	\$1,607,798
124a	Row River Road from Sharps Creek Road to Brice Creek Road	Construct to major collector standards with two 11' travel lanes and 6' shoulders on both sides. Integrate system safety measures.	County	\$2,190,750	\$0	\$127,000	\$695,325	\$254,000	\$139,065	\$3,406,140	\$3,739,750	\$5,235,651
124c	Row River Road from Snauer Lane to Laying Creek Road	Construct safety improvements including shoulder rumble strips and fixed object removal.	County	\$414,000	\$0	\$24,000	\$131,400	\$48,000	\$26,280	\$643,680	\$706,724	\$989,414
90b	Jasper Road from S 42nd to northwest of Mt. Vernon Road	Modify Jasper Road to a three-lane cross-section with sidewalks and bicycle facilities.	County/City of Springfield	\$4,580,000	\$0	\$175,000	\$1,426,500	\$540,000	\$285,300	\$7,006,800	\$7,693,073	\$10,770,302



AGENDA COVER MEMO

Memorandum Date: February 21, 2019

Meeting Date: March 12, 2019

TO: Board of County Commissioners

DEPARTMENT: Public Works

PRESENTED BY: Dan Hurley, Public Works Director
Peggy Keppler, County Engineer
Orin Schumacher, Road Maintenance Manager

AGENDA ITEM TITLE: DISCUSSION / Local Access Roads

I. MOTION

None. Discussion only

II. AGENDA ITEM SUMMARY

Lane County has two types of public roads under its jurisdiction: County Roads and Local Access Roads. The degree of County responsibility for these roads depends on applicable State and County regulations. County Roads are maintained by the County using Road Fund resources. Local Access Roads (LARs) are not maintained by Lane County. Maintenance responsibility for LARs falls to the property owners along these roads. Many of the LARs in Lane County are in need of significant maintenance or repair. The intent of this work session is to provide an overview of current policies, to highlight examples of maintenance challenges, and to explore the Board's desire to address issues or make policy changes.

III. BACKGROUND/IMPLICATIONS OF ACTION

A. Board Action and Other History

There are approximately 129 miles of LARs in Lane County. These roadways were built over many decades, typically privately, to gain access to one or more properties. Over time, these roads became public roads as a matter of public record but they have not been accepted as County Roads into the maintained County Road system. Most of these roads are sub-standard with respect to the County's Road Design Standards for County Roads.

The County regulates LARs in a limited way in order to provide basic safety to Lane County citizens. Lane Code Chapter 15 specifies minimum design requirements, signage requirements, setback distances, and other minimum standards. The County does not require facility permits for any work in the right-of-way on these roads.

On infrequent occasions, the Board has authorized the use of Road Funds for maintenance or repairs on specific LARs to address emergency conditions. However, the ability to spend Road Fund dollars on LARs is restricted, as discussed below, by state statutes.

B. Policy Issues

Oregon Revised Statute 368.031 states that a Local Access Road is open to the public and is “*subject to the exercise of jurisdiction by a county governing body in the same manner as a county road except as follows:*”

(1) A county and its officers, employees or agents are not liable for failure to improve the local access road or keep it in repair.

*(2) A county governing body shall spend county moneys on the local access road **only if** it determines that the work is an emergency or if:*

(a) The county road official recommends the expenditure;

(b) The public use of the road justifies the expenditure proposed; and

(c) The county governing body enacts an order or resolution authorizing the work and designating the work to be either a single project or a continuing program.”

Lane Code 15.076 (1)(b) aligns with the state statute as follows:

“(b) Pursuant to Oregon Revised Statutes (ORS) 368.031, the County may spend money on Local Access Road improvements only under limited circumstances, and only by order or resolution by the Board.”

C. Board Goals

The (2018-2021) Strategic Plan lists Robust Infrastructure as one of its four Strategic Priorities: *Focus on strategic infrastructure maintenance and investments that have the highest return for safety, vibrant communities, and long term environmental benefit.*

Key Strategic Initiatives under this priority include:

a. Enhance safe transportation facilities and operations

- b. Maintain existing facilities and identify efficiencies in capital assets*
- c. Fund and develop new facilities that support safety and livability*

D. Financial and/or Resource Considerations

Lane County does not have resources in the existing Road Fund budget to maintain LARs. The Lane County Road & Bridge Maintenance audit, completed by the County's Performance Auditor in 2017, noted that current funding and preservation on County roads and bridges falls short of what is needed to protect these assets.

Prior to 2011, the County maintained the County road infrastructure (excluding LAR's) largely with combined funds from federal timber payments and state highway allocation revenue. This combined revenue was \$35 million in FY 07-08. However, between 2008 and 2017 federal Secure Rural Schools payments declined by nearly \$21 million. A state transportation funding package passed in 2009 and implemented over a two year period in 2010 and 2011 provided an increase in State Highway Revenue allocation of \$7 million, bringing the combined revenue to \$21 million in FY 16-17. These two significant revenue changes brought a net decrease of \$13 million (37%) over ten years.

In 2017, the state legislature passed a second transportation funding package (HB2017) that will restore a portion of lost funding over a period of ten years, but this will not restore the funding levels of FY 07-08 previously available to maintain the County's roads. New funding from HB 2017 is expected to grow from \$2.5 million in 2018 to \$11 million in 2027.

Lane County's Road Fund is used for ongoing maintenance by the Road Maintenance Division (approximately \$19 million annually) and for capital projects through the Engineering and Construction Services Division (approximately \$16 million annually). New funding from HB 2017 has enabled Public Works to increase chip seal miles, restore a bridge crew, implement safety improvements, increase engineering capacity, increase bridge maintenance and preservation and add to paving projects.

Capital projects for road and bridge improvements are selected from the County's Transportation System Plan (TSP). The TSP reflects the County's transportation needs over a 20-year period. The current TSP was adopted in 2017 and included projects in three categorizations: Currently Funded Projects, Financially Constrained Projects, and Illustrative Projects. Currently Funded Projects include \$83.7 million in transportation improvements that would be designed and constructed by Lane County in partnerships with ODOT and local jurisdictions. Financially Constrained Projects include 72 projects within the County that could be reasonably funded over a 20-year period by leveraging state and federal funding with Lane County Road Funds. The anticipated cost of these 72 projects is roughly \$261.6 million. The Illustrative Project list includes \$748.5 million of transportation solutions identified as County and ODOT improvements needed within the County that are not reasonably likely to be funded before 2036. With an annual Capital

Improvement budget of \$4.5 million, it will take decades to address these known transportation needs. A single project can often exceed the County’s annual allocation of funds so staff apply for state and federal funding as it becomes available.

The cost to repair or maintain all of the Local Access Roads in Lane County is not currently known and is not included in the Road Fund Budget. Lane County does not maintain an inventory of LARs by road types or road conditions. The minimum design criteria for LAR’s required by Lane Code 15.706 is 2” Asphalt Concrete on 10” Crushed Rock when serving 4 or more properties. If a LAR was to be rebuilt with County funds to these specifications, the cost is estimated to be \$340,000 per mile.

E. Health Implications

Safety	Without periodic maintenance, roadway pavement conditions deteriorate and can create safety hazards for drivers.
Transportation	Local Access Roads are public roads that provide access to jobs, services, and educational resources.

F. Analysis

Local Access Roads present a major funding dilemma both for the County and for residents served by these roads. Road repairs are often extremely costly and can be beyond the financial means of the adjacent property owners. Formation of a Local Improvement District (LID) provides a method by which improvements can be made on a LAR with the cost of the improvements being assessed against the benefitting properties property owners, but the formation of an LID can be cumbersome and may take years to address issues.

Lane County is responsible for maintaining over 1,400 miles of roadway in Lane County and more than 415 bridges. These roads and bridges are expensive to maintain and the Road Fund has not been sufficiently resourced to maintain existing assets. HB 2017 will provide much needed resources to preserve existing roadways and limited funds for Capital Improvement Projects, but no funding has been identified for upgrades or repairs to the 129 miles of Local Access Roads (LAR’s) in Lane County.

Without investment into the maintenance of LARs by benefitting property owners or Lane County, these roads will continue to deteriorate and will continue to propose safety hazards to users of these public roadways. The current slide on Collard Lake Road is a typical example of the challenges posed by LARs. This road was never built to standards for a County Road, utility work (not requiring a permit) has likely compromised the roadway, and now part of the road is failing. Since the road is a public road, property owners along the roadway do not

understand why the County isn't addressing the maintenance issues on the road.

In 1998, the Board of Lane County Commissioners authorized \$150,000 in funding for one-time emergency repairs on Collard Lake Road and authorized the expenditure of additional Road Funds as necessary to provide for a continuing program of limited maintenance of the road following its repair. The current failure is beyond the scope of limited maintenance. Staff engineers are currently evaluating the area and developing a cost estimate for the repair.

G. Alternatives/Options

None. Discussion only.

IV. RECOMMENDATION

Additional analysis is needed to present options for the Board. Staff recommends conducting further investigation on the condition and types of LARs, frequency of public use, and costs estimates for maintenance needs. Additional data could help prioritize the use of limited funds if authorized in the future by the Board.

V. TIMING/IMPLEMENTATION

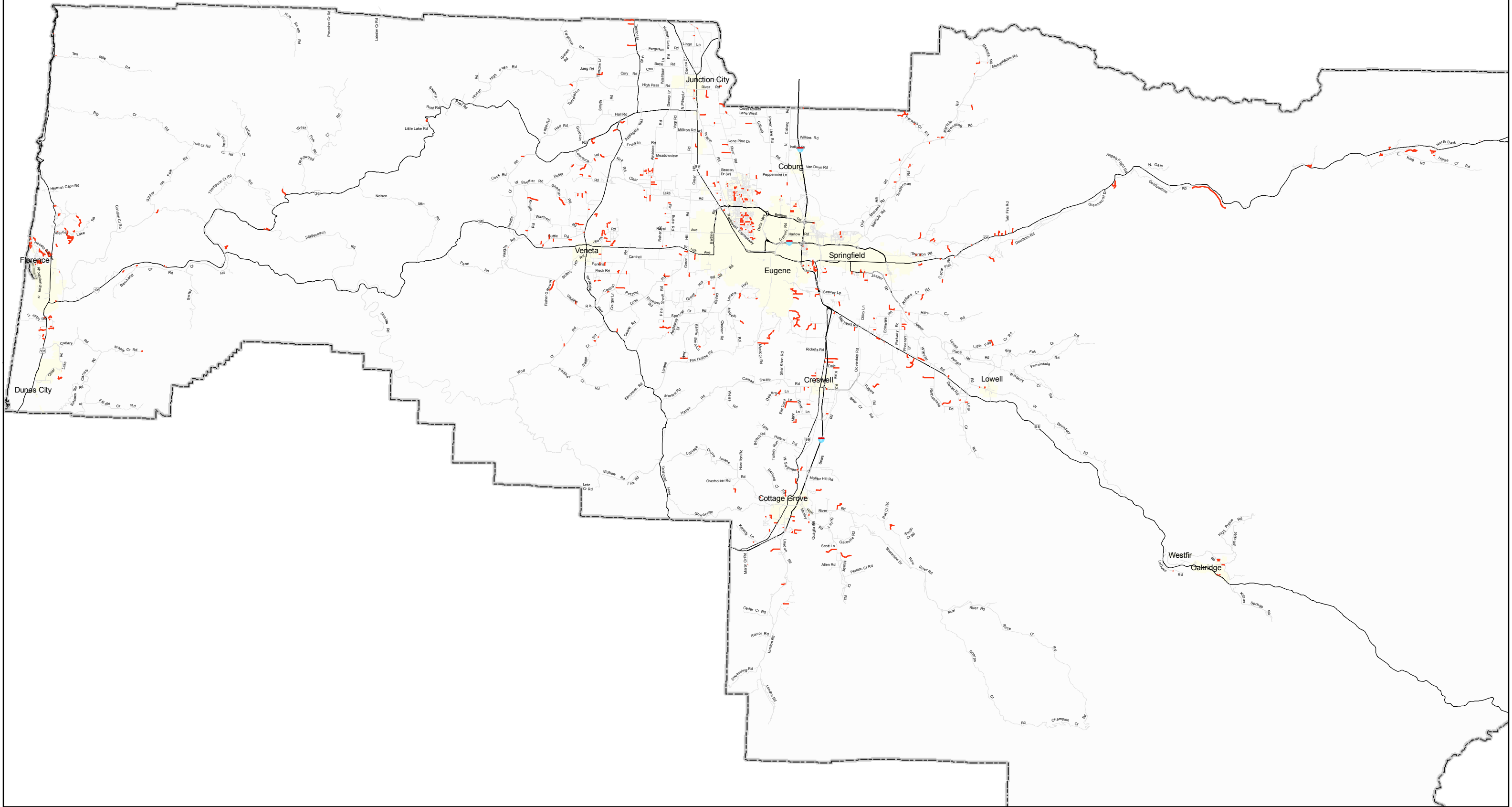
To be determined based on Board direction.

VI. FOLLOW-UP

To be determined based on Board direction.

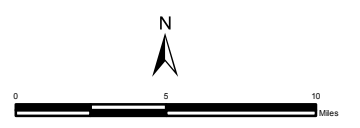
VII. ATTACHMENTS

Map of County LARs



The information on this map was derived from digital databases on the Lane County regional geographic information system. Care was taken in the creation of this map, but is provided "as is". Lane County cannot accept any responsibility for errors, omissions or positional accuracy in the digital data or the underlying records. Current plan designation, zoning, etc., for specific parcels should be confirmed with the appropriate agency. There are no warranties, expressed or implied, accompanying this product. However, notification of any errors will be appreciated.

- LAR
- County Road
- State Road
- City Limits



Lane County, Oregon
 Drawn By: xxx

Local Access Roads

Local Access Roads – Policy Discussion

The BCC has asked staff and the Transportation Advisory Committee (TrAC) to investigate changing County policies around LARs. This question is twofold:

A. Should we change how we treat existing LARs?

1. Are there different considerations for LARs that are outside of City UGBs vs. inside City UGBs (see summary of LAR miles within Cities' UGBs)?
2. Is there a tipping point when there are so many parcels served by an LAR or trips taken on the LAR that it should be taken into the County Road System?
 - a. What are the best measures to determine the tipping point?
 - b. Does it need to have 50 feet of ROW dedication before it can be brought in?
 - c. Does it need to be brought up to County Road standards before being brought in?
 - i. If so, who shoulders the burden of the upgrade?
3. Should we consider requiring facility permits on LARs?
4. Assuming that we cannot require property owners fronting existing LARs to form a special road district, a local improvement district, a home owners association, or sign a maintenance agreement with the County, is there something we can do to incentivize some of these options?

B. Should we change if and how future LARs are created?

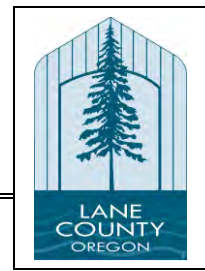
1. Is there any way to ensure home buyers are aware of their maintenance obligation for the LAR?
2. Is there a point during formation of a LAR that the County requires some sort of assurance that the LAR will be maintained?
 - a. Require a maintenance agreement with the County?
 - b. Require a special road district?
 - c. Require a Local Improvement District?
3. Should we stop allowing the formation of LARs all together?

Are there other options/questions to consider?



Lane County Department of Public Works
Road Management Information System
Local Access Road City Summary

<u>City Name</u>	<u>City Mileage</u>
Cottage Grove	0.251
Creswell	0.023
Eugene	0.649
Florence	0.247
Junction City	0.024
Lowell	0.210
Oakridge	0.517
Springfield	0.680
Total City Miles	2.601
LAR Mileage Outside City	124.964
Total LAR Miles	127.565



TO: Transportation Advisory Committee (TrAC)

DEPARTMENT: Public Works

PRESENTED BY: Kerry Werner, Engineering Design Program Manager
Becky Taylor, Senior Transportation Planner

AGENDA ITEM: Territorial Highway Reconstruction Project

I. ACTION

No formal action is requested of the TrAC. This is an informational item to prepare the TrAC for a public hearing on this project at the July 24, 2019 meeting. The public hearing is pursuant to Lane Manual 15.580 which establishes a public involvement process and requires Design Concept approval by the Lane County Board of Commissioners. The attached draft Territorial Highway (Gillespie Corners – Town of Lorane) Corridor Plan constitutes the Design Concept.

II. BACKGROUND

For years, Lane County has been working with the community to improve Territorial Highway, especially the portion located between Gillespie Corners and the town of Lorane, in response to the tragic death of Jane Higdon who was hit by a logging truck on her bicycle. Until only recently, Territorial Highway has been under the jurisdiction of the Oregon Department of Transportation, making it a lower priority for investments when compared to other State facilities, such as Beltline and Interstate-5. On March 21, 2019, Territorial Highway was transferred to Lane County which now has ownership and jurisdiction over the entire corridor; maintenance responsibilities, however, will be phased over several years.

The jurisdictional transfer of Territorial Highway to Lane County came with additional funding provided by the Oregon Legislature. There is not enough funding to improve the entire corridor, and investments will be prioritized toward maintenance and safety. A portion of the corridor, between the communities of Gillespie Corners and Lorane, has committed funding for improvements which is the focus of this project.

The design concept for this project was shaped by a community planning effort which began in 2014. Construction of the community-preferred alternative is now possible and is scheduled to commence in 2020 and be phased over several years through 2024. The project is to improve the 5.71 miles of Territorial Highway located between Gillespie Corners (milepost 32.06) and the town of Lorane (milepost 37.37). The improvements will generally follow the existing roadway alignment, with the exception of the segment through the slide area (milepost 34.09 to 35.3 at Stony Point) and include:

- Widening the road to provide 6-foot shoulders and two 11-foot vehicle travel lanes
- Softening 4 curves to improve access and sight distance
- Replacing culverts and raising and widening bridges to address deficient waterway crossings
- Stabilizing the slide at Stony Point through roadway realignment

III. RECOMMENDATION / NEXT STEPS

A public meeting is scheduled for June 20, 2019 to update stakeholders on the development and implementation of the project. TrAC members are welcome to attend the June 20 public meeting, between 6:00 and 8:00 p.m., at the Lorane Grange, 80342 Old Lorane Road.

A public hearing before the Lane County Transportation Advisory Committee (TrAC) is tentatively scheduled for July 24, 2019. The TrAC will advance a recommendation on the Design Concept (attached Corridor Plan) to the Board of County Commissioners for a Board Order.

IV. FOR MORE INFORMATION

Feel free to contact Becky Taylor by phone at 541-682-6932 or by email at becky.taylor@co.lane.or.us.

Also visit the project webpage at www.lanecounty.org/territorialhighway

V. ATTACHMENTS

Draft Design Concept: Territorial Highway (Gillespie Corners – Town of Lorane) Corridor Plan

taken in early days of the (Crossed 1853)
Roller Mountain family at Smithfield, Oregon
now known as Franklin - taken on the -
now known as Groves mark farm across
from Cemetery those present are George,
Minnie, Albert, Henry Rollin, Ira
owned by Warren S. Mounts - Grandson



Territorial Highway (Gillespie Corners - Town of Lorane) Corridor Plan and Design Concept

DRAFT May 2019

Planned improvements and design concepts for the Territorial Highway corridor located between Gillespie Corners and the town of Lorane

Acknowledgements

In Honor of Jane Higdon

So many community members rallied around the tragic death of Jane Higdon to bring about this project

Special thanks to the following stakeholders for their involvement and leadership

Congressman Peter DeFazio
Senator Floyd Prozanski
Senator Ron Wyden
Senator Jeffrey A. Merkley
Representative Paul Holvey
Jay Bozievich, Lane County Commissioner – District 1 West Lane
Sid Leiken, Former Lane County Commissioner
Richard Hughes, Bicycle Advocate
Tom Jefferson, Widower of Jane Higdon
Ed King, King Estates

Regional Partners

Oregon Transportation Commission
Oregon Department of Transportation
Lane Area Commission on Transportation

Project Management and Technical Team:

Kerry Werner, Project Manager and Design Engineer
Kevin Brown, Environmental
Peggy Keppler, County Engineer
Alex Cuyler, Legislative Liaison
Becky Taylor, Transportation Planner

Thanks to the legacy and hard work of prior staff:

Lydia Kaye (McKinney), former project manager
Bill Morgan, former County Engineer
Janet Mayer, former design engineer
Peder Alison, former engineering technician
Sarah Wilkinson, former planner

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Appendix 1. Design Drawings

Executive Summary

Territorial Highway is a valuable asset, as a continuous corridor through West Lane County, providing regional connectivity to adjacent counties to the north and south, and local access to rural communities and resource lands, including forest, farming, and winery operations. The City of Veneta and several unincorporated communities, such as Cheshire, Elmira, Crow, Hadleyville, and Gillespie Corners and Lorane, have developed around Territorial Highway. It has as a rich culture, as part of the historic Applegate Trail, and is a gateway to the scenic countryside, making it a popular bicycle route and tourist attraction.

The highway was built without the benefit of current technology and equipment, yet it intelligently followed the natural terrain. As such, it is a narrow, windy road, presenting challenges for roadway users. The needs and values of the highway vary along the corridor, depending on the surrounding environment, which include residential and commercial communities, as well as farmlands and wetlands.

The consistent need throughout the corridor is improvements to the roadway, as the current structure is deteriorated with potholes. There are active landslides along the corridor with areas of roadway failure. Several bridges and culverts flood on a regular basis and waters overtop the roadway during 100-year storm events, preventing through traffic.

For years, Lane County has been working with the community to improve Territorial Highway, especially the portion located between Gillespie Corners and the town of Lorane, in response to the tragic death of Jane Higdon who was hit by a logging truck on her bicycle. Until only recently, Territorial Highway has been under the jurisdiction of the Oregon Department of Transportation, making it a lower priority for investments when compared to other State facilities, such as Beltline and Interstate-5. On March 21, 2019, Territorial Highway was transferred to Lane County which now has ownership and jurisdiction over the entire corridor; maintenance responsibilities, however, will be phased over several years.

The jurisdictional transfer of Territorial Highway to Lane County came with additional funding provided by the Oregon Legislature. There is not enough funding to improve the entire corridor, but investments will be prioritized toward maintenance and safety. A portion of the corridor, between the communities of Gillespie Corners and Lorane, has committed funding for improvements which is the focus of this plan.

This plan provides the design concept that was shaped by a community planning effort which began in 2014. Construction of the community-preferred alternative is now possible and is scheduled to commence in 2020 and phased over several years through 2024. The project is to improve the 5.71 miles of Territorial Highway located between Gillespie Corners (milepost 32.06) and the town of

Lorane (milepost 37.37), generally following the existing roadway alignment, with the exception of the segment through the slide area (milepost 34.09 to 35.3 at Stony Point) and including:

- Widening the road to provide 6-foot shoulders and two 11-foot vehicle travel lanes
- Softening 4 curves to improve access and sight distance
- Replacing culverts and raising and widening bridges to address deficient waterway crossings
- Stabilizing the slide at Stony Point through roadway realignment

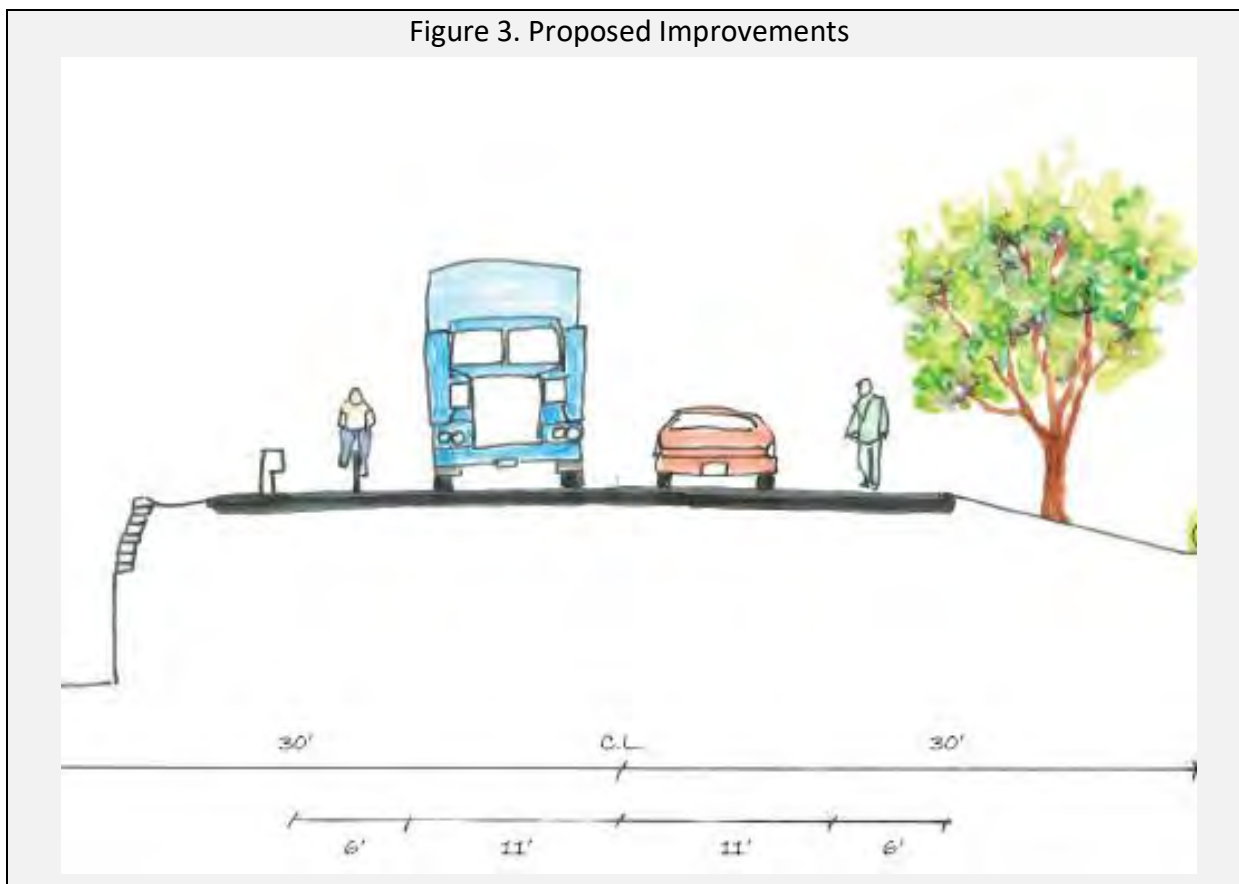
Figure 1. Solution Study



Lane County Staff Evaluate Roadway

Plan

The improvements – the preferred design alternative that was supported by the community – include widening the paved surface to 34 feet in width, to provide two 11-foot travel lanes and six-foot shoulders. Improvements include addressing both vertical and horizontal curves and raising and widening bridges that frequently flood. The project will also stabilize the roadway on Stony Point by realignment. Design details are provided in Chapter 5.



Process

This plan constitutes a Design Concept, pursuant to Lane Manual 15.580 which establishes a public involvement process and requires Design Concept approval by the Lane County Board of Commissioners. Consistent with these procedural requirements, there was a robust public involvement process to determine the appropriate design solution. Lane County has been working with stakeholders for several years to develop this project.

2014-2015

Lane County initiated the planning process in 2014, upon receipt of a \$440,000 Transportation, Community, and System Preservation Program (TCSP) grant, which was matched with \$100,000 in private funds and in-kind staff time from Lane County and ODOT. Lane County's successful grant application was supported by over 60 stakeholders, including community members, emergency

service providers, and members of the bicycling community, and logging and trucking industries. The 2014-2015 process included public workshops and resulted in a community preferred alternative which is reflected in this Design Concept. The public involvement process is detailed in Chapter 3.

2016-2019

Following the public workshops, Lane County staff investigated the feasibility of the community-preferred design. The investigation included geotechnical, hydraulic, pavement, right-of-way, utilities, storm water, cultural and environmental analysis. Although the design was intended to minimize impacts, by generally following the existing alignment, the physical characteristics of wetlands and an active slide necessitated additional funding to advance the design, prompting Lane County to apply for Statewide Transportation Improvement Program (STIP) funding. In 2016, Lane County produced a technical report summarizing the findings, concluding the planning funding. An additional \$1M STIP funding for design was programmed for 2018-2021.

In 2017, the Oregon Legislature provided \$30M for the jurisdictional transfer of Territorial Highway from the Oregon Department of Transportation to Lane County. The funding is not enough to improve the entire highway, but enabled Lane County to commit to constructing the proposed improvements for Territorial between Gillespie Corners and the town of Lorane. In 2018 and 2019, Lane County completed the design work necessary to ensure the feasibility of constructing the community-preferred alternative.

On June 20, 2019, a public meeting is planned to update stakeholders on the development and implementation of the project. A public hearing before the Lane County Transportation Advisory Committee (TrAC) is tentatively scheduled for July 24, 2019. The TrAC will advance a recommendation on the Design Concept to the Board of County Commissioners for a Board Order.

2020-2024

Approval of this plan and Design Concept will enable Lane County staff to advance the project toward construction, which is planned to commence in phases between 2020 and 2024 as shown in Figure 4. The first priority for construction is stabilizing Stony Point which is scheduled to commence in 2020 and involves a realignment that will soften sharp curves. Phase 2 construction in 2021 is at the north end of the project at Gillespie Corners which will involve replacing two bridges. Phase 3 construction in 2022 is between Easy Acres Drive and Hamm Road which includes realignment of a curve. The final phase of construction is planned to commence in 2023 to complete the southern portion of the project to the town of Lorane. The entire corridor, from Gillespie Corners to Lorane, will be improved to have six-foot wide shoulders.

Figure 4. Construction Phasing Plan



Chapter 2 – Existing Conditions

This chapter describes constraints and opportunities of the corridor today.

Roadway Conditions

The average daily traffic (ADT) of Territorial Highway is 1700 vehicle trips and conveys a relatively high percentage of freight traffic (10%). Bicyclists utilize this scenic rolling highway for recreational and commuter purposes, although there is no bike lane or shoulder. The overall roadway width varies between 20 and 22 feet. The roadway geometry is substandard with sharp curves and steep grades that limit sight distance. A portion of the project is located within an active slide area at Stony Point and will require geotechnical mitigation. Bridges within the project area are prone to flooding and need to be raised or replaced.

Figure 5. Existing Roadway Conditions



No shoulders for bicycles



Poor Pavement



Slides and Sharp Curves

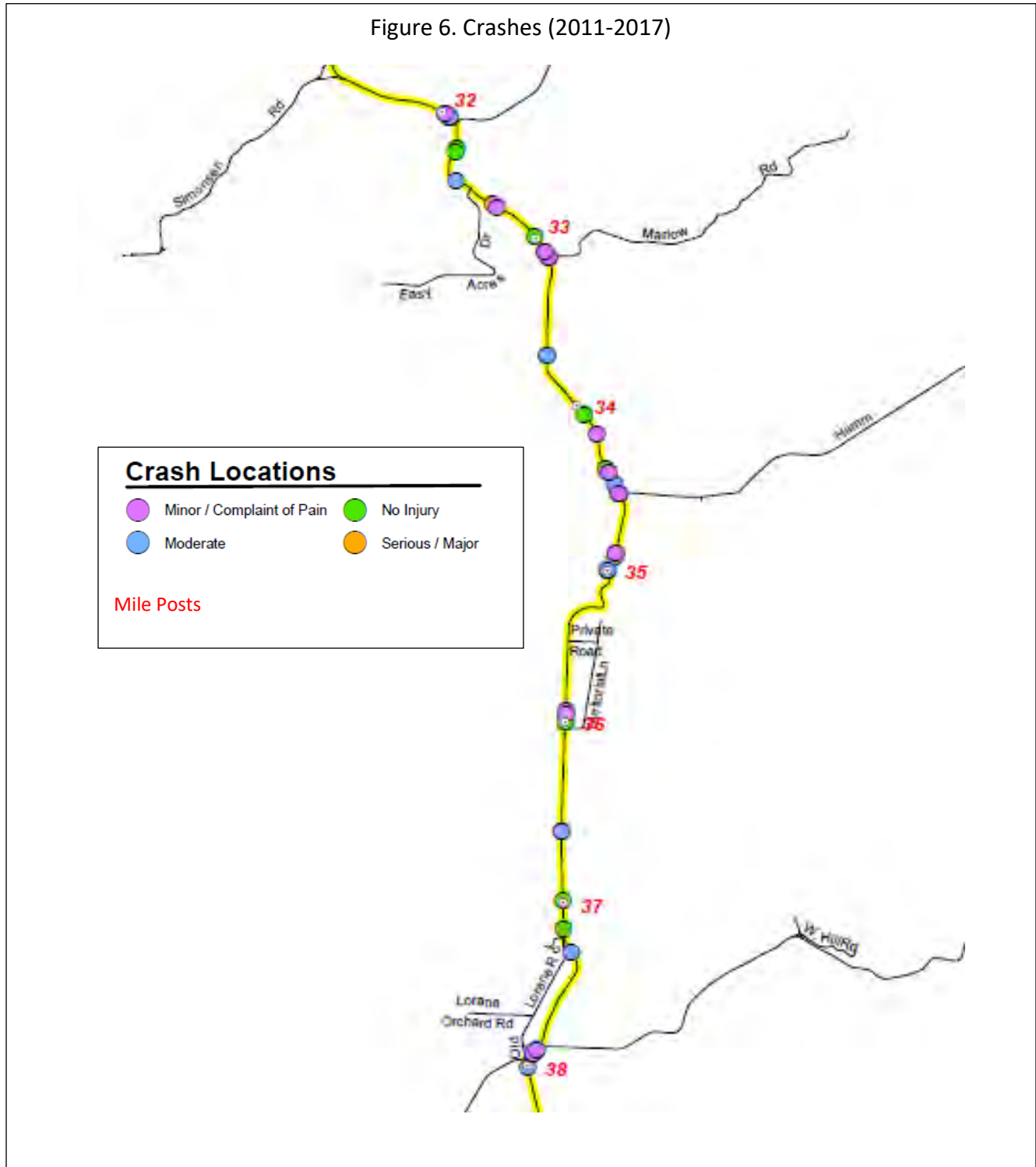


Flooding

Safety

This project was initiated following the tragic death of Jane Higdon, who was hit by a logging truck while cycling on Territorial Highway. As shown in Figure 6, there have been 24 crashes along the corridor, resulting in a variety of injuries in recent years.

Figure 6. Crashes (2011-2017)



Environmental

There are known cultural, wetland, waterway and habitat resources around the project area. Prior to the construction of any improvements, a more detailed review of environmental impacts will occur, in accordance with the National Environmental Policy Act (NEPA) or other applicable regulations. A summary of resources is provided below.

Cultural Resources: Archaeological and Historical Sites

Territorial Highway, which runs the full length of Lane County, was once the path of the historic Applegate Trail used by pioneers as a southern approach to the rich fertile land of the Willamette Valley. A preliminary subsurface reconnaissance of the project site by an ODOT Archaeologist resulted in the identification of three sites containing prehistoric or historic artifacts. The sites were on private property outside of the existing right-of-way. Although permission to enter was granted by the property owners for the exploration, the location and findings are considered sensitive to protect both the property owners and the artifacts. Remains found at the isolated finds were considered to be too sparse to warrant further investigation. The project management team evaluated the alignment of the various alternatives to avoid impact to these sites.

A historic building exists at the southern end of the project. The structure was built in 1920 and was once a tavern, known as the “Dew Drop.” The building is currently vacant. The property owners shared some of the history with staff and expressed a willingness to work with the County to preserve this building. The building will need to be moved to implement the project.

Figure 7. Historic Structure



The Dew Drop Tavern (Closed)

Geology: Ancient and Active Landslide(s)

An ancient and active landslide exists adjacent to Territorial Highway, where it abuts Stony Point. The roadway has obvious sinking and sliding damage. Roadway repairs and slide stabilization work has been minimal.

Review of existing information dated back to 1951 with an Oil and Gas Investigations Map. Aerial photography was compared over several decades, beginning with 1952. State drawings of the roadway were also consulted. ODOT staff has multiple recorded site visits, from 1983 to present.

Between 2014 and 2015, ODOT took instrumentation readings within the project area, which detected movement, resulting in a preliminary geotechnical report for the project. More recently, Lane County has hired geotechnical professionals to collect additional data and develop geotechnical recommendations necessary to build the project.

Water Resources: Waterways and Wetlands

The project spans two river watershed basins: the Long Tom and the Siuslaw. Coho salmon, which is listed as threatened under ESA, are found in the Siuslaw watershed. The Long Tom watershed provides habitat for cutthroat trout, which is listed as a species of concern by the Oregon Department of Fish and Wildlife. Coyote Creek, Norris Creek, Crow Creek, North Fork Siuslaw River, and three unnamed creeks are within the project area and are considered “waters of the United States” and “waters of the State.” As such, any construction activity below the Ordinary High Water Mark requires authorization from the Department of State Lands and the US Army Corps of Engineers, in accordance with Oregon’s Removal-Fill Law and the Clean Water Act.



Wetlands are located along the length of the project. A wetland delineation report was prepared by Lane County and submitted to the Department of State Lands. Approximately 7.4 acres of wetlands may be impacted by this project. Impacts to wetlands needs to minimized and mitigated in order to comply with federal and state requirements. Any runoff to the wetlands requires pre-treatment to reduce pollutants.

Runoff from the roadway currently sheet flows to roadside ditches and open waterways. The surrounding soils are predominantly silty clay loams, which are classified as hydrologic soil group C. Group C soils have low infiltration rates when thoroughly wetted and consist chiefly of soils with a layer that impedes downward movement of water and soils with moderately-fine to fine structure. If stormwater runoff from the roadway is to infiltrate, the adjacent soils will need to be amended by replacing the existing poor-draining soils.

The area currently floods and two bridges and several culvert structures are substandard. Preliminary hydraulics study has been performed by ODOT at both bridges, but a more detailed analysis is underway. The current rise required for the bridge raisings based on preliminary data is 2'-6". This value may change and may be different for each bridge. The final hydraulics study will reveal this information.

Approximately eighty culverts are located within the limits of the project. All of the culverts have a diameter of 4 feet or less and can be designed to small culvert criteria as outlined in ODOT’s Hydraulic Manual with the exception of three culverts located at possible fish bearing stream crossings. These three culverts may require a medium culvert design study and may need to meet either NMFS or ODFW fish passage standards, further study is required.

Chapter 3 – Public Involvement

The community provided significant input on the project which is summarized in this chapter.

This plan was developed through a robust public involvement process, which brought the diverse viewpoints of stakeholders together. Community support is demonstrated by the \$100,000.00 private cash donations and over 60 letters of support. The Jane Higdon Foundation provided \$87,500. King Estates Winery pledged \$12,500. Support letters were provided from wineries, logging companies, the school district, cyclists, residents, and business owners.

Stakeholder Input

Stakeholders, representing a variety of interests, were consulted to review and comment on the data. The Stakeholder Advisory Committee included the following members:

- Jim Bailor, Lorane Rural Fire Protection District
- Catherine Boucher, Local Resident (Hamm Road)
- Theresa Brand, Lane Transit District (LTD) Point2Point
- Jed Kaul, Long Tom Watershed Council
- Dean Livelybrooks, Crow-Applegate-Lorane School District
- Bill McCoy, Freight
- Ellen Mooney, Lane County Roads Advisory Committee (former)
- Terry Ney, Lane Fire Authority
- Paul Nicholson, Bicycle Way of Life
- John Norrena, Lane Electric
- Ann Sanders, Siuslaw Watershed Council
- Arite Weiner, King Estate Wine

Public Meetings

On October 28, 2014, the first open house was held at King Estates, which is within the project boundary. Over 100 people participated in this event, representing diverse users, including freight, bikers, emergency service providers, and area residents. There was strong consensus that addressing safety while maintaining the rural character of the road should be the primary goal.



Figure 10. Public Meeting #2



June 16, 2014, Lorane Grange

A second open house was held on June 16, 2014 at the Lorane Grange to view and discuss two design alternatives that were developed based on the public input received at the first open house. The project team received strong public support to improve Territorial Highway to a 34-foot wide road that mostly maintains the existing road alignment. In the preferred alternative, travel lanes would be widened, shoulders added, horizontal and vertical curves realigned, and new bridges to raise the road and eliminate repeat flood events.

A third public meeting will be held on June 20, 2019 to update stakeholders on the project.

Public Hearing

The TrAC is scheduled to conduct a public hearing on July 24, 2019.

Project Webpage

Throughout the process, the County updated a webpage dedicated to the study to enable interested parties to review key documents, be informed about upcoming opportunities to provide in-person feedback and to establish a County staff member as the primary contact point for the process. Through this information, a number of interested residents and business owners contacted County staff to provide feedback independent of formal meetings or briefings.

Mailings

Interested parties received project updates via email. Postcards and letters were mailed to abutting property owners at project milestones, such as the jurisdictional transfer and notices of public meetings.

Environmental Justice and Title VI Civil Rights

Environmental Justice (EJ) and Title VI focus on understanding and addressing the unique needs of different socioeconomic groups, which are vital components to effective transportation decision-making. Key areas of consideration for compliance include: identifying populations so that their needs can be acknowledged and addressed; and evaluating and improving the public involvement process to eliminate participation barriers and engaging minority and low-income populations in transportation decision-making.

Potential environmental justice populations surrounding the project area include: residents over 65. There were no discernable minority, limited English proficient, impoverished or disable populations for the affected census tracts. Direct mail and holding public meetings nearby are outreach procedures supportive of elderly needs. Both of the techniques were used throughout the process.

Chapter 4 – Alternatives Considered and Preferred Alternative

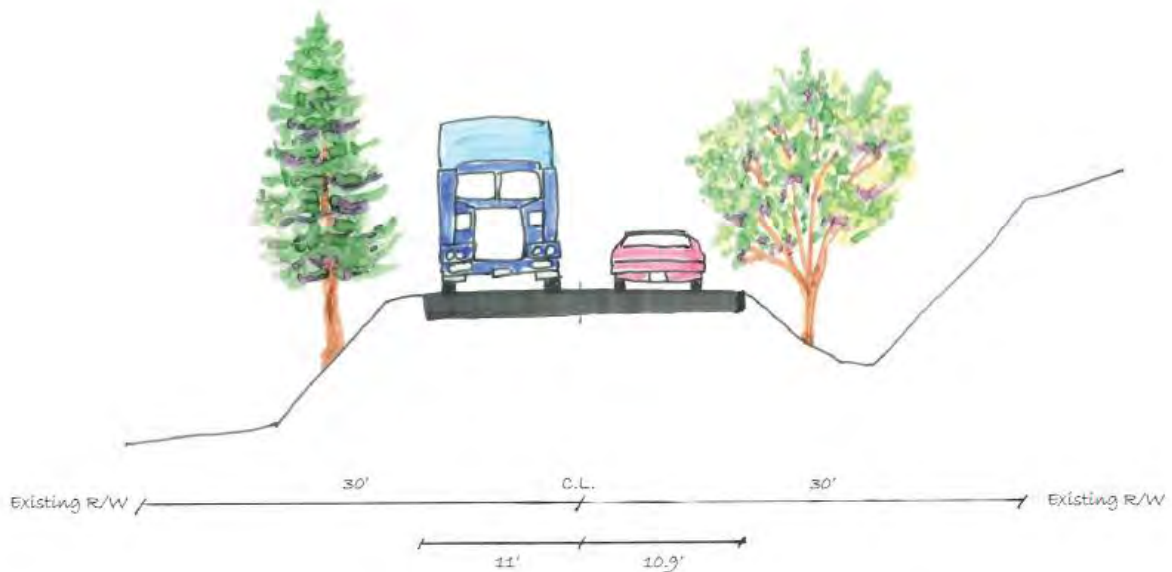
This chapter describes the outcome of the planning process.

Alternatives Considered

Following completion of the existing conditions analyses and stakeholder interviews, staff reviewed the opportunities and constraints associated with alternatives for the corridor. These alternatives were vetted through two public open houses, resulting in a clear community preference for a context sensitive design that minimizes environmental impacts and maximizes use of the existing roadway footprint. The community specifically preferred the slowest design speeds on the curves and a roadway cross-section with 11-foot wide vehicle travel lanes (one in each direction) and six-foot wide shoulders (on both sides).

- 1. Do Nothing:** The existing conditions (see Chapter 2) are unsafe and will only worsen over time. The option to do nothing is not acceptable.

Figure 11. Existing Cross-Section



Typical Section - Existing

- 2. **Build Traditional Cross-Section (ODOT/AASHTO 4R Standards):** This design facilitates 55 mph speeds throughout the corridor, requiring significant changes to horizontal and vertical geometry, and includes 12' travel lanes and 8' shoulders, creating a wider footprint impacting adjacent properties and natural resources. This alternative was not supported by the community which valued the natural surroundings, historical character, and slower vehicle speeds.

Figure 12. Traditional Cross-Section

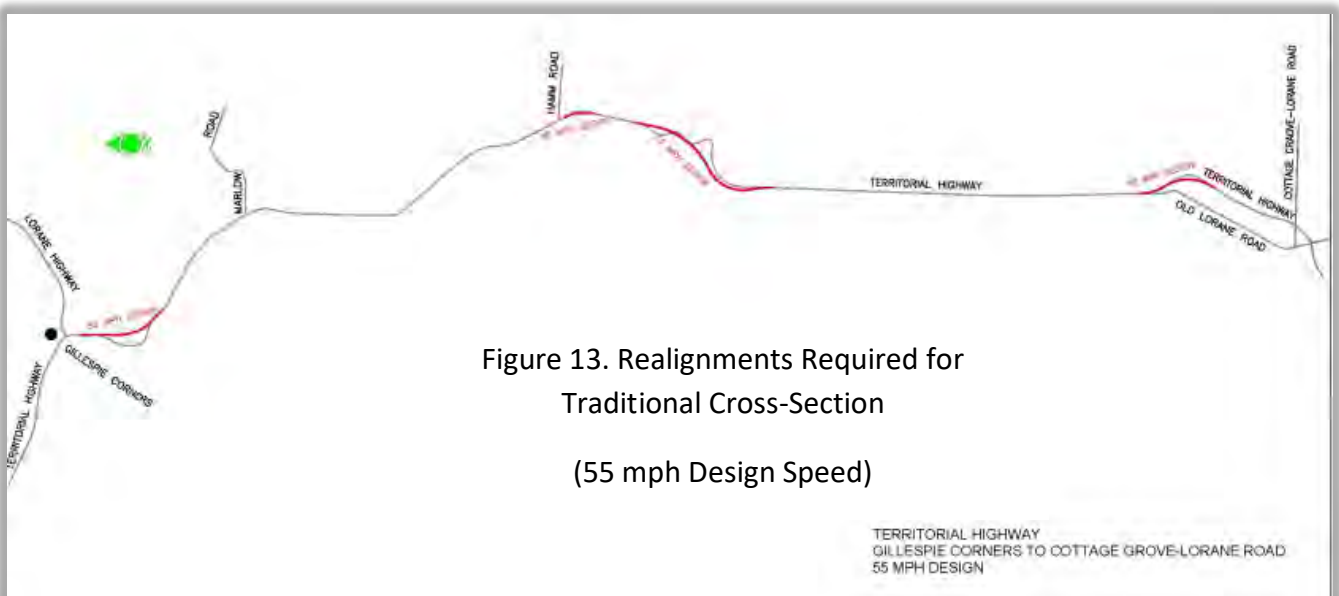
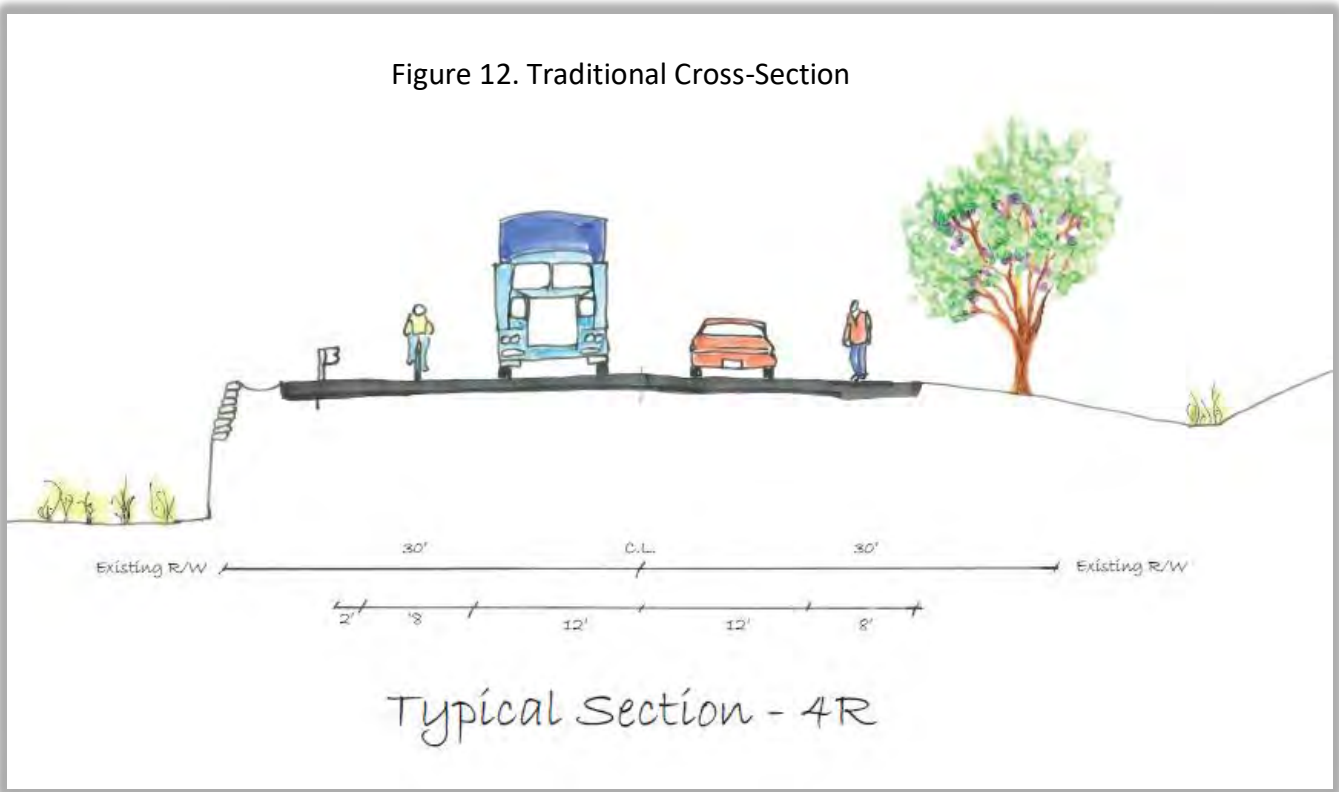
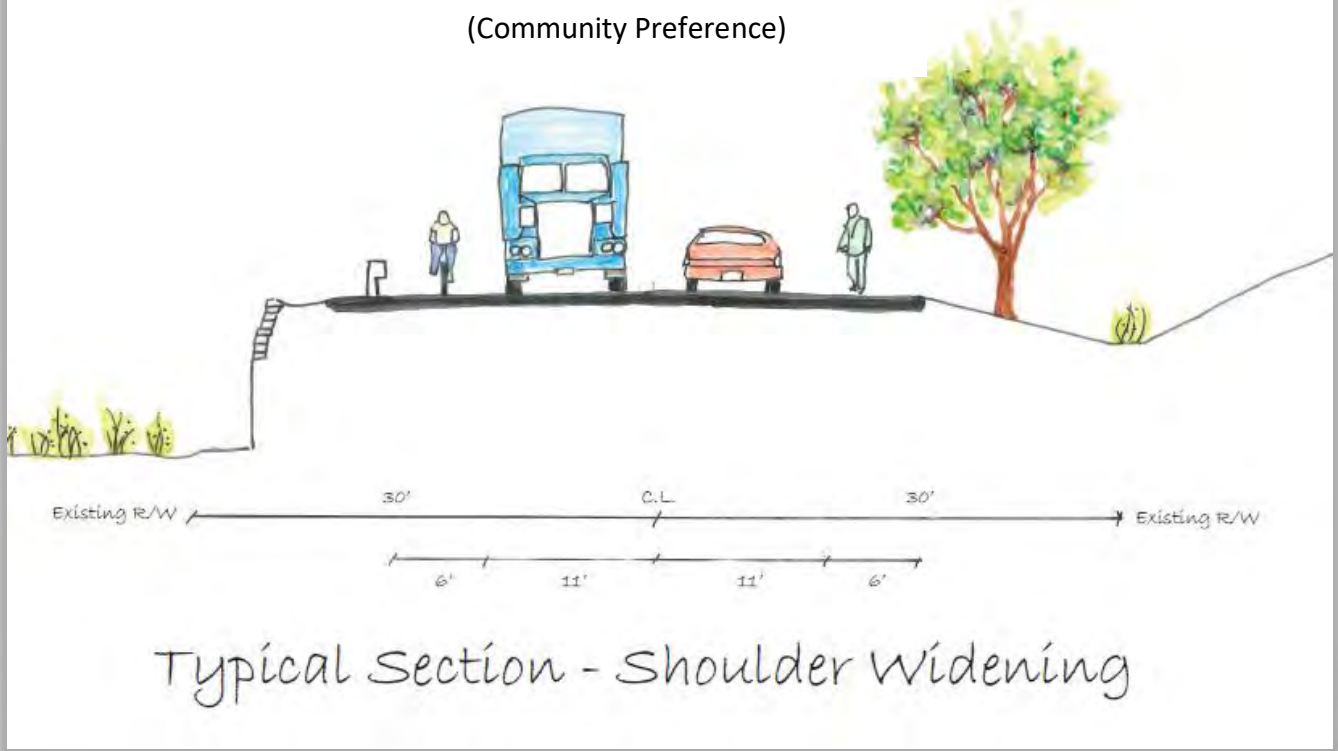


Figure 13. Realignments Required for Traditional Cross-Section (55 mph Design Speed)

3. **Build Context Sensitive** (ODOT/AASHTO 3R Standard): The community-preferred design is to generally follow the existing roadway alignment to minimize environmental impacts and to facilitate slower vehicle speeds, resulting in the softening of sharp curves rather than significant realignments. The preferred travel lane configuration is 11-foot lanes in each direction for vehicles and six-foot shoulders on both sides for people to walk and bicycle.

Figure 14. Context-Sensitive Cross-Section
(Community Preference)



A comparison of the traditional design and context-sensitive designs are illustrated in the following figures showing three design speed alignments of 55 mph, 45 mph, and 35 mph. The faster speeds require more significant roadway realignments and involve more property impacts. The community preference was for the slowest design speed which also more closely follows the existing roadway alignment.

Figure 16. Comparison of Design Speed Alignments (Mid-Section)

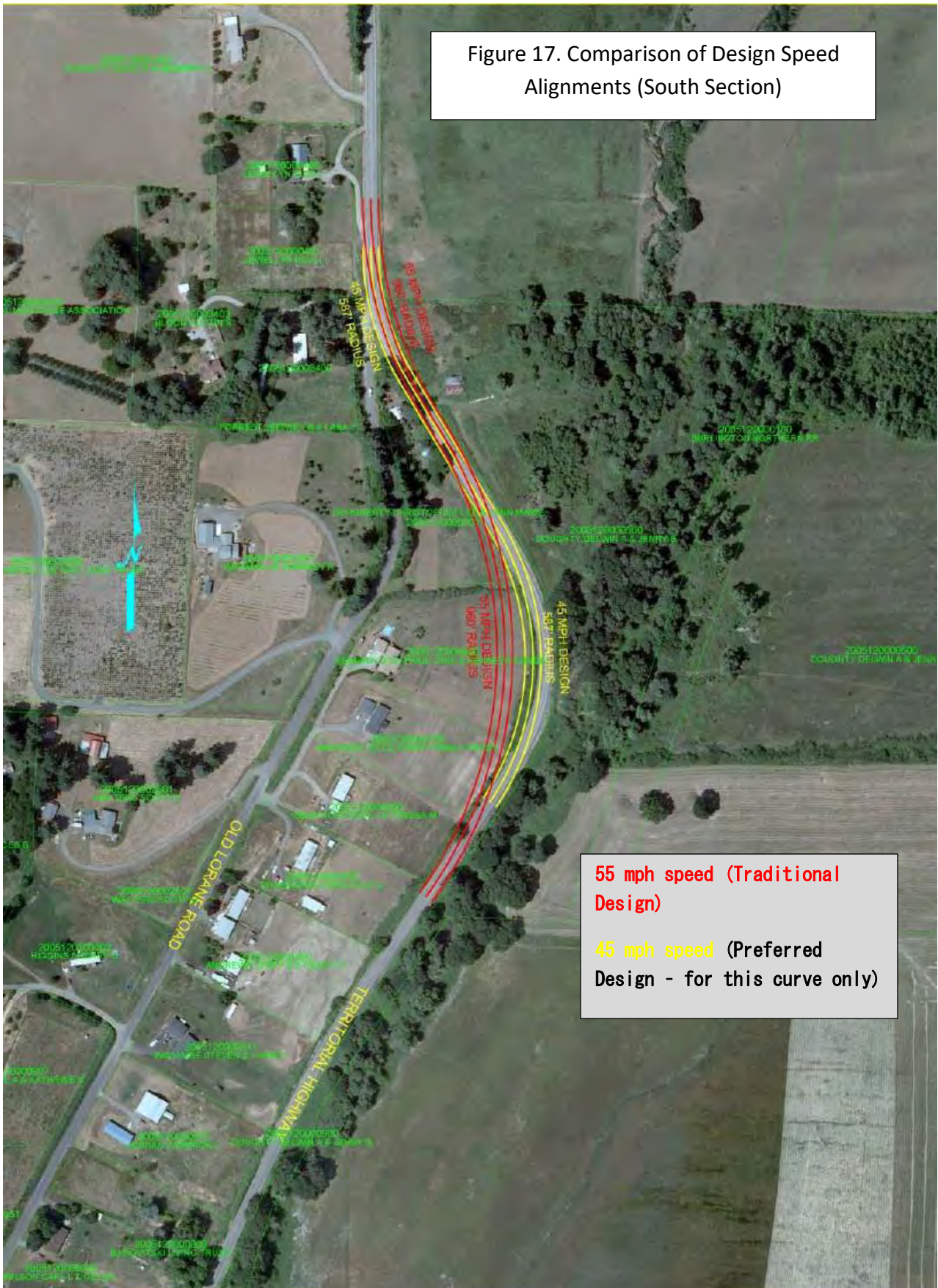


55 mph speed
(Traditional Design)

45 mph speed
(Variable Design)

35 mph speed
(Preferred Design)

Figure 17. Comparison of Design Speed Alignments (South Section)



55 mph speed (Traditional Design)
45 mph speed (Preferred Design - for this curve only)

Chapter 5 – Design Concept

This chapter provides the design drawings consistent with the community-preferred alternative which constitutes the Design Concept.

Design Drawings

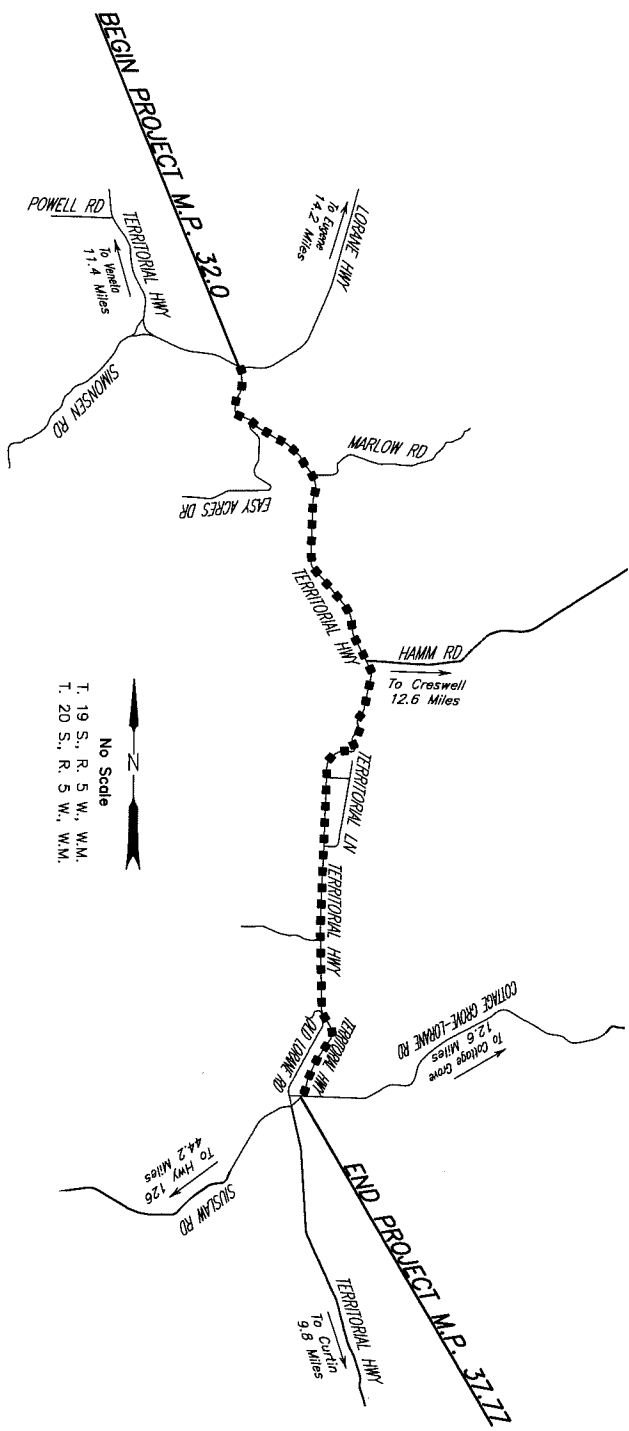
The design drawings are included as Appendix 1. These drawings translate the community-preferred alternative into a design concept for the project. The project is to improve the 5.71 miles of Territorial Highway located between Gillespie Corners (milepost 32.06) and the town of Lorane (milepost 37.37), generally following the existing roadway alignment, with the exception of the segment through the slide area (milepost 34.09 to 35.3 at Stony Point) and including:

- Widening the road to provide 6-foot shoulders and two 11-foot vehicle travel lanes
- Softening 4 curves to improve access and sight distance
- Replacing culverts and raising and widening bridges to address deficient waterway crossings
- Stabilizing the slide at Stony Point through roadway realignment

The design concept is required to be approved by the Lane County Board of Commissioners. The plans will then advance into more detailed engineering drawings for construction. Affected property owners will be consulted during the design refinement, right-of-way acquisition, and construction process in order to minimize and mitigate property impacts.

Approval of this plan and Design Concept will enable Lane County staff to advance the project toward construction, which is planned to commence in phases between 2020 and 2024 as shown in Figure 4. The first priority for construction is stabilizing Stony Point which is scheduled to commence in 2020 and involves a realignment that will soften sharp curves. Phase 2 construction in 2021 is at the north end of the project at Gillespie Corners which will involve replacing two bridges. Phase 3 construction in 2022 is between Easy Acres Drive and Hamm Road which includes realignment of a curve. The final phase of construction is planned to commence in 2023 to complete the southern portion of the project to the town of Lorane. The entire corridor, from Gillespie Corners to Lorane, will be improved to have six-foot wide shoulders.

Appendix 1. Design Drawings



No Scale
 T. 19 S., R. 5 W., W.M.
 T. 20 S., R. 5 W., W.M.

VICINITY MAP

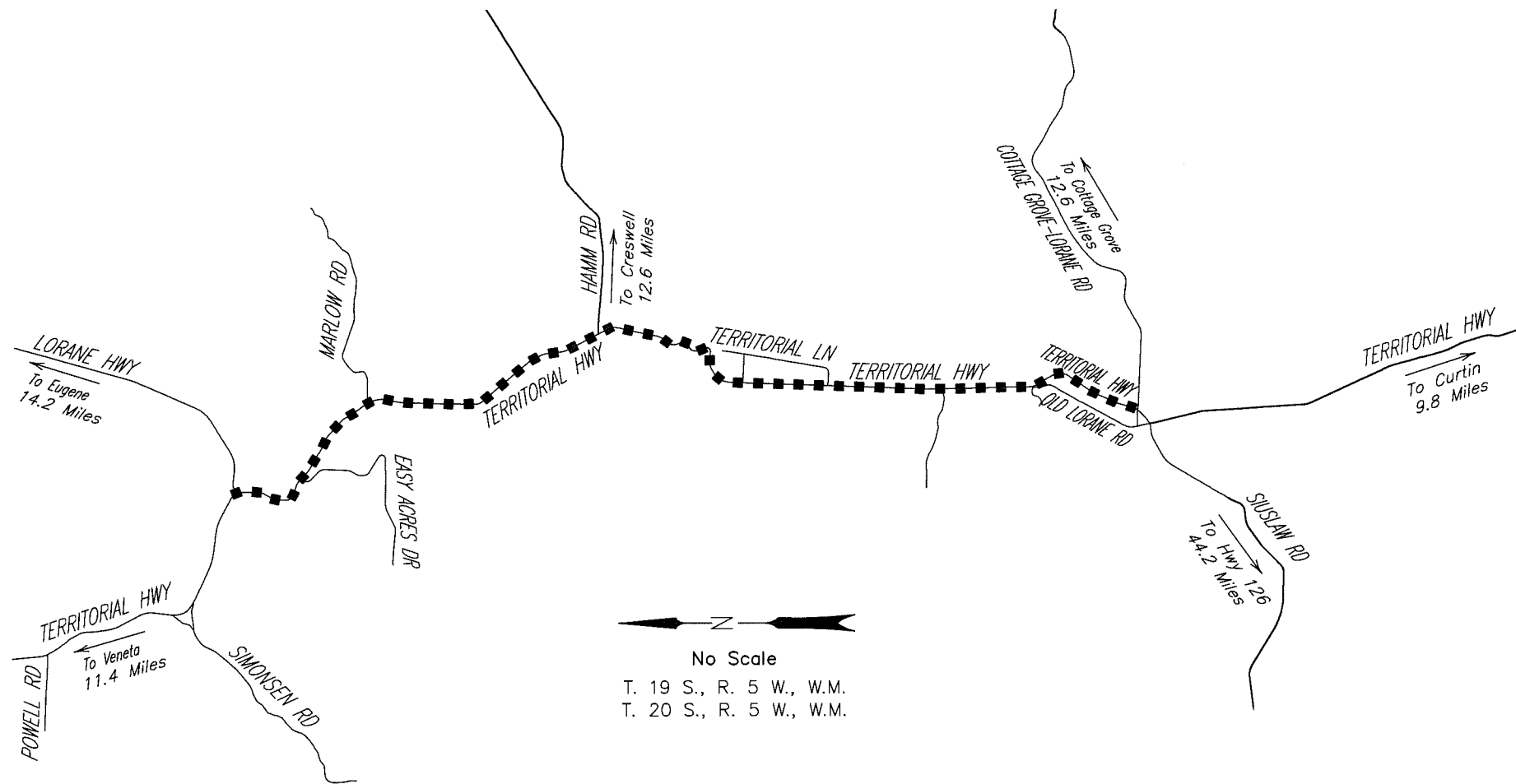
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	May 22, 2019 TrAC meeting - page 43				
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			LANE COUNTY DEPARTMENT OF PUBLIC WORKS ENGINEERING AND CONSTRUCTION SERVICES DIVISION MARSHA A. MILLER Public Works Director		WILLIAM F. MORGAN, P.E. County Engineer



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 PLANS FOR PROPOSED PROJECT
GRADING, DRAINAGE, STRUCTURES AND PAVING
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

XXXXXXX 20XX

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2C Thru 2C-X	Traffic Control Plans
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3	Construction Notes
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11B	Profile
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12A	General Construction (Plan)
12B	Profile



Overall Length Of Project - 5.77 Miles

ATTENTION
 Oregon Law Requires You To Follow Rules
 Adopted By The Oregon Utility Notification Center.
 Those Rules Are Set Forth In OAR 952-001-0010 Through
 OAR 952-001-0090. You May Obtain Copies Of The Rules From The Center.
 The Telephone Number For The Utility Notification Center is 811 Or 1-800-332-2344

PUBLIC WORKS DIRECTOR

MARSHA A. MILLER

COUNTY ENGINEER

WILLIAM F. MORGAN P.E.

LANE COUNTY COMMISSIONERS

JAY BOZIEVICH, CHAIR
 FAYE H. STEWART II, VICE CHAIR
 PAT FARR
 SID LEIKEN
 PETER SORENSON

CONTRACT NO. XX/XX-XX

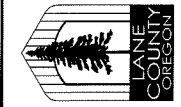
ROAD MAINT. NO.

PROJECT NO. STP-XXXX(XXX)

PRELIMINARY Feb 02, 2016



Standard Drg. Nos.



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION
 WILLIAM E. MORGAN, P.E.
 County Engineer
 MARSHA A. MILLER
 Public Works Director

Appr'd	Revision	Date

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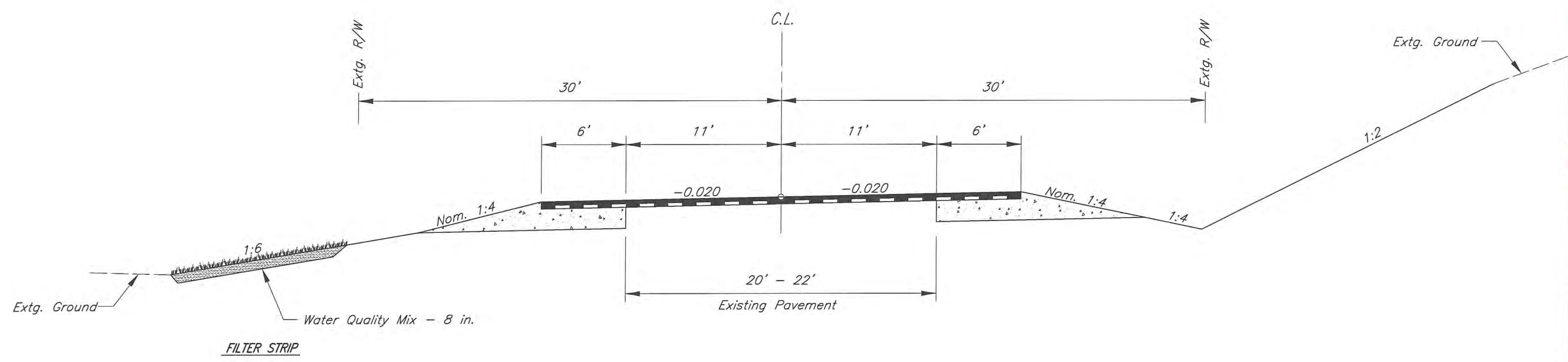
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22	Construction Notes
22A	General Construction (Plan)
22B	Profile
23	Construction Notes
23A	General Construction (Plan)
23B	Profile
GA Thru GA-X	Erosion Control Plans
GN Thru GN-X	Planting Plans
ST-1 Thru ST-X	Permanent Pavement Markings and Signs

SHEET NO. 1A



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN
 County Engineer



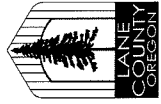
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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	201X		

SHEET NO. 2



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION
 WILLIAM F. MORGAN
 County Engineer

MARSHIA A. MILLER
 Public Works Director

Appr'd

Revision

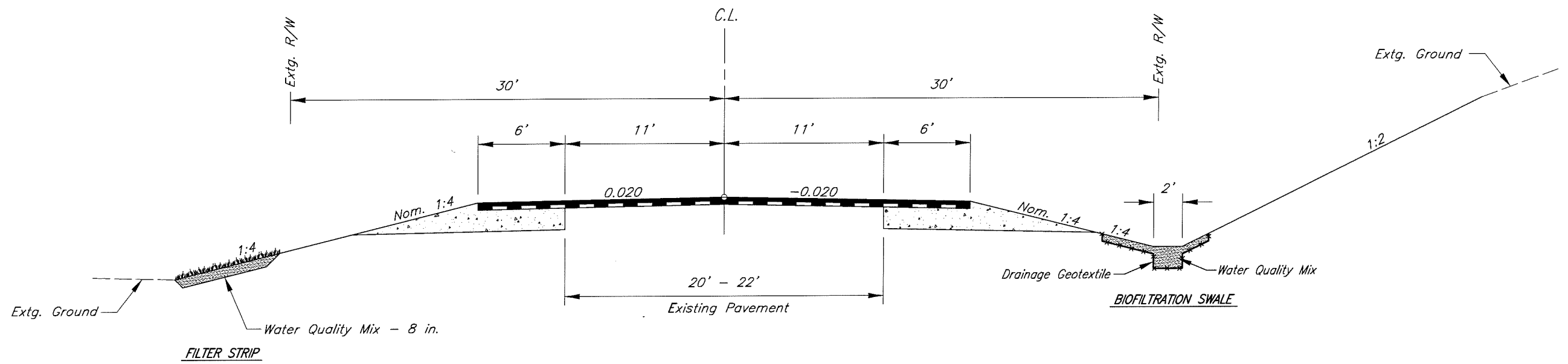
Date

Filename: topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

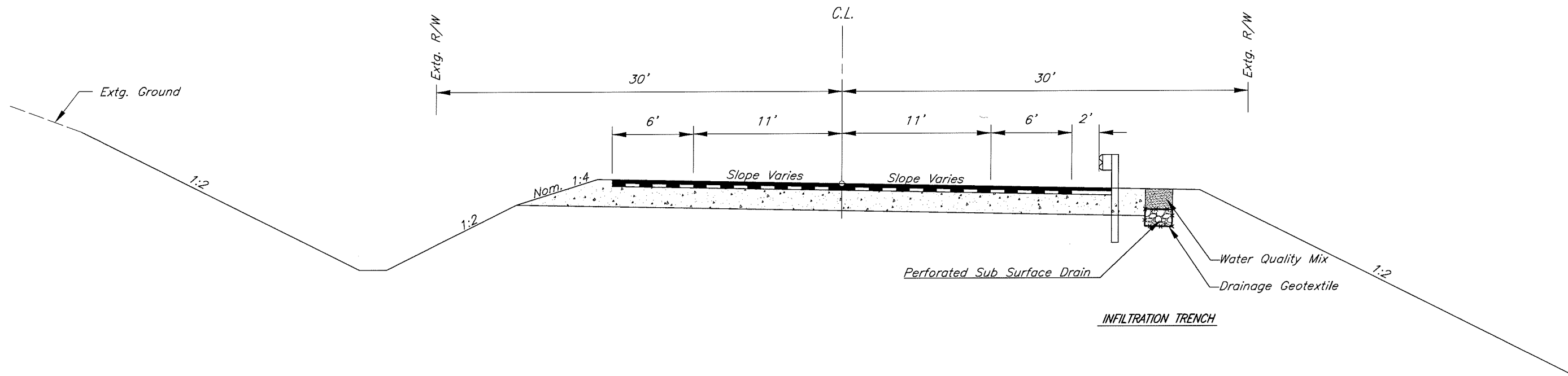
SHEET NO. 2A



NORMAL CROWN WITH BIOFILTRATION SWALE



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN
 County Engineer

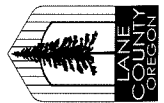


SUPERELEVATED WITH INFILTRATION TRENCH

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 2A-2



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
WILLIAM F. MORGAN
County Engineer

MARSHA A. MILLER
Public Works Director

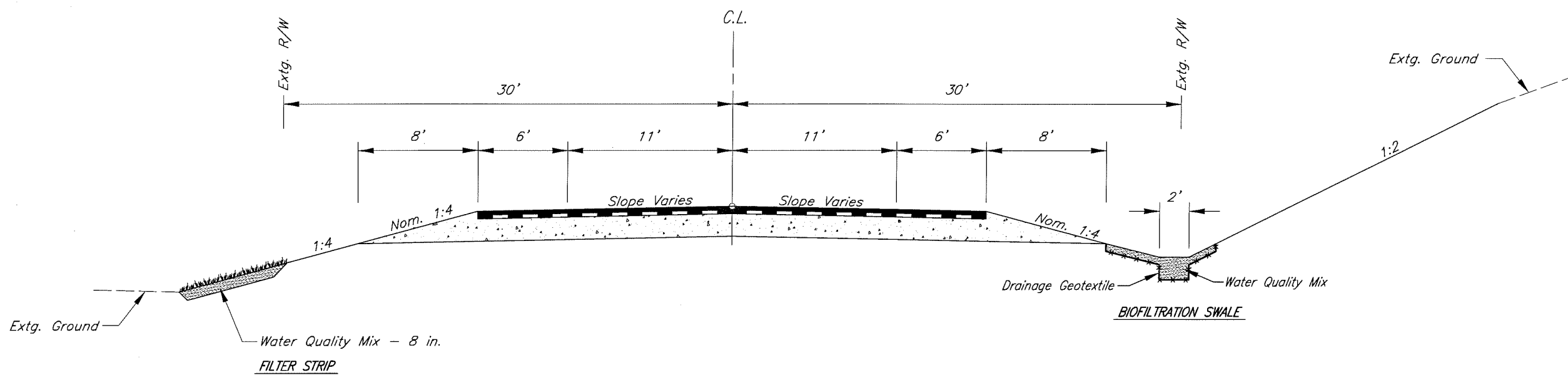
Appr'd	
Revision	
Date	

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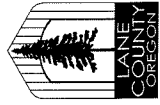
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX

SHEET NO. 2A-3



FULL RECONSTRUCT



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Appr'd	Revision	Date

Filename: H:\Projects\17989-02\DES\Plan&Profile\Note.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

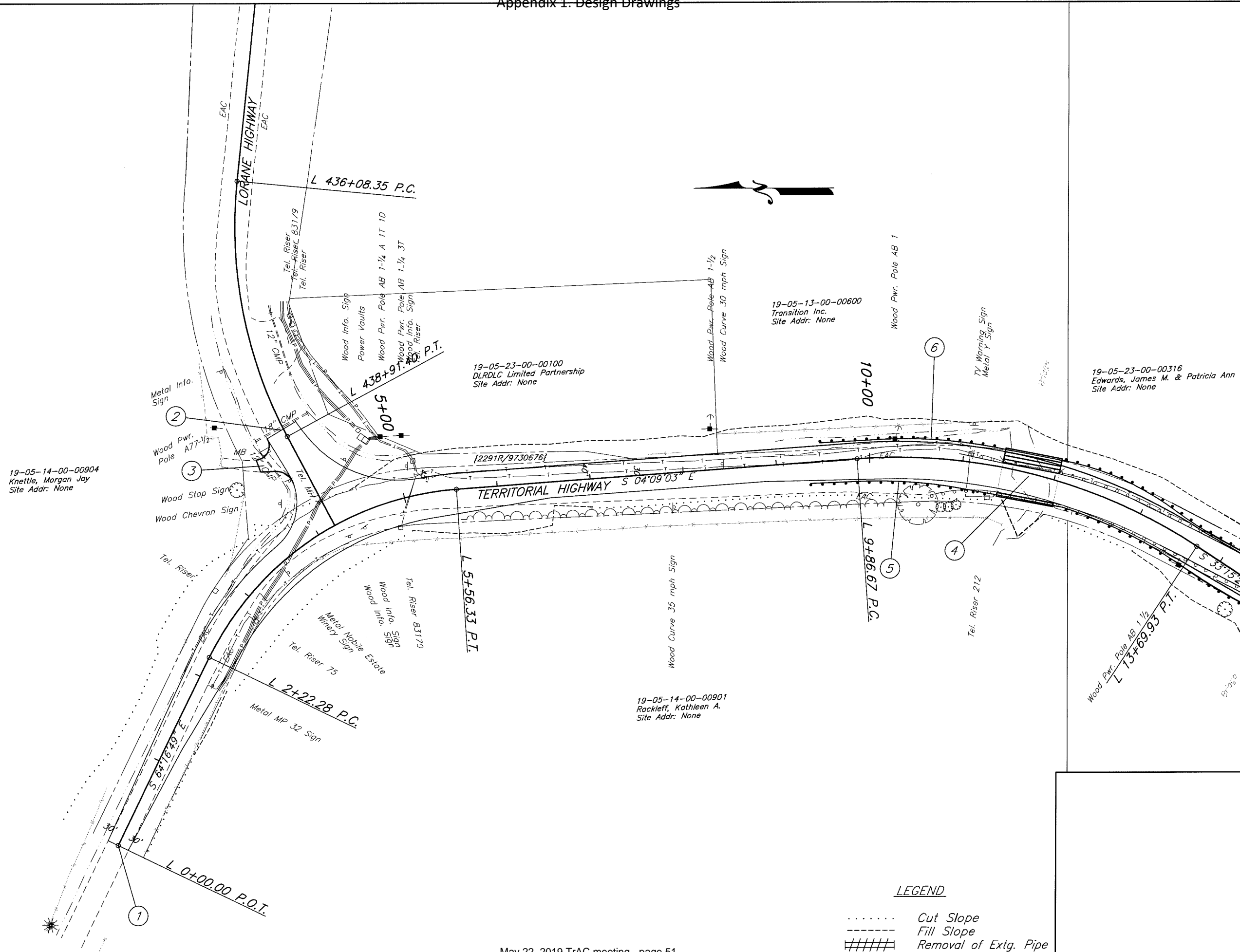
DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX

SHEET NO. 3

- ① Sta. L 0+00.00, Lt. & Rt.
Sawcut And Match Extg Pavement
Begin Total Reconstruction
- ② Sta. L 438+82.15, Lt. & Rt.
Sawcut And Match Extg Pavement
- ③ Sta. L 439+04.44 Rt.
Const. Asph. Conc. Drwy. - 13.8' Wide
(For Details, See Dwg. RD715)
- ④ Sta. L 11+73.55 Lt. & Rt.
Raise Extg 32'x62' Bridge - x.x'
Widen Extg 32'x62' Bridge - 4 Slabs Lt. & 1 Slab Rt.
(For Details, See Sht. xx)
- ⑤ Sta. L 9+35.42 Rt. To Sta. L 11+42.55 Rt.
Const. Guardrail, Type 2A - 125 Ft.
Const. Guardrail, Type 3 - 12.5 Ft.
Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
W=1.0', E=2.0', L=50', Test Level 3
Const. Guardrail Transition - 1 Each
(For Details, See Dwg. RD400, RD405, RD415 and RD420)
- ⑥ Sta. L 9+48.20 Lt. To Sta. L 11+42.90 Lt.
Const. Guardrail, Type 2A - 125 Ft.
Const. Guardrail, Type 3 - 12.5 Ft.
Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
W=1.0', E=2.0', L=50', Test Level 3
Const. Guardrail Transition - 1 Each

Legend

- Cut Catch Line
- Fill Catch Line



19-05-14-00-00904
Knettle, Morgan Jay
Site Addr: None

19-05-23-00-00100
DLRDLC Limited Partnership
Site Addr: None

19-05-13-00-00600
Transition Inc.
Site Addr: None

19-05-23-00-00316
Edwards, James M. & Patricia Ann
Site Addr: None

19-05-14-00-00901
Rocklett, Kathleen A.
Site Addr: None

LEGEND

- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHIA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

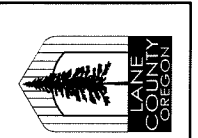
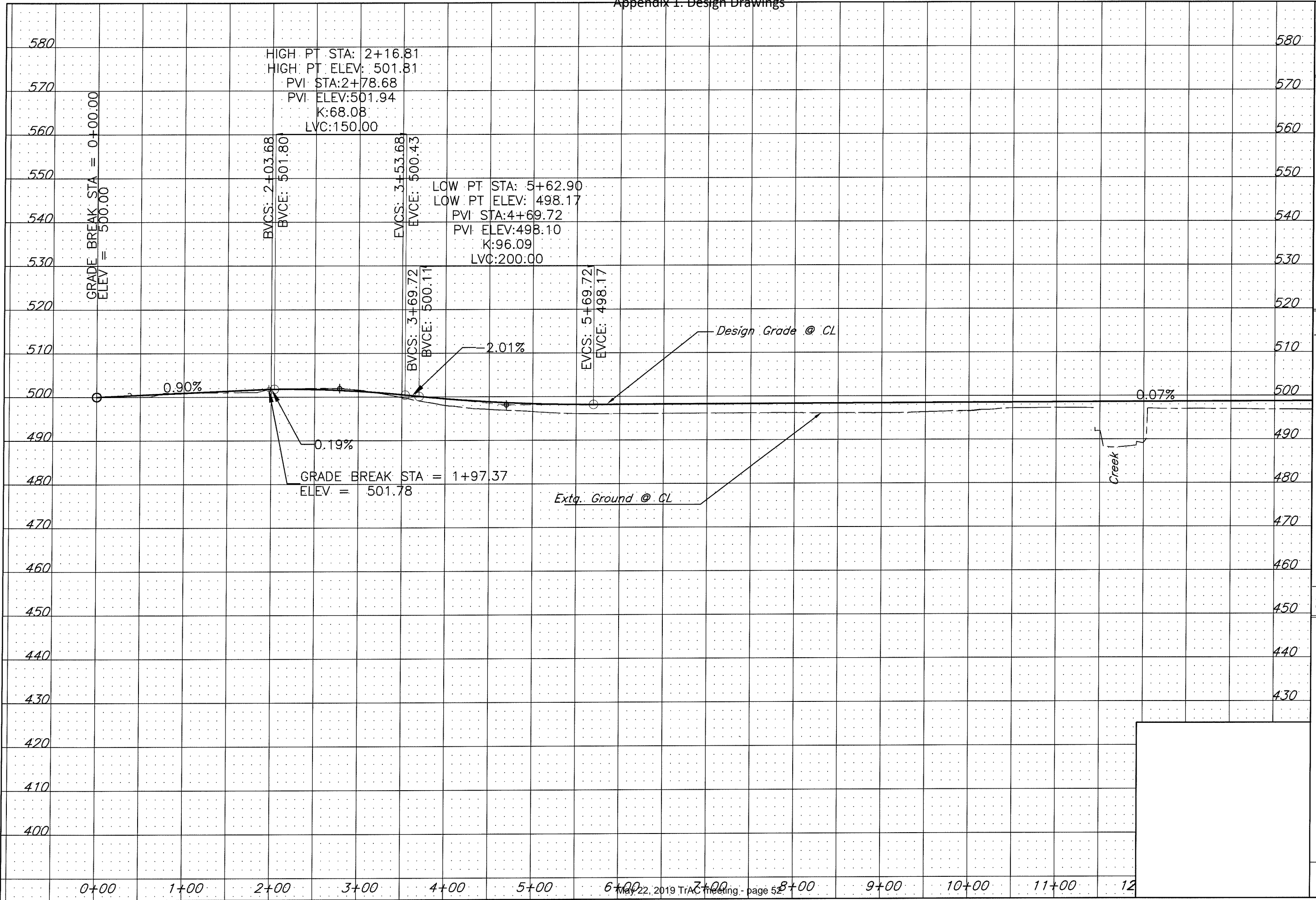


Date	Revision	App'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 3A

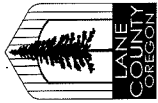


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
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 STATE HWY. NO.: XXXXXX



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 WILLIAM F. MORGAN, P.E.
 County Engineer

MARSHA A. MILLER
 Public Works Director

Date	Revision	Appr'd

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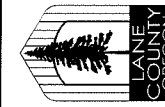
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
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 STATE HWY. NO.: XXXXXX

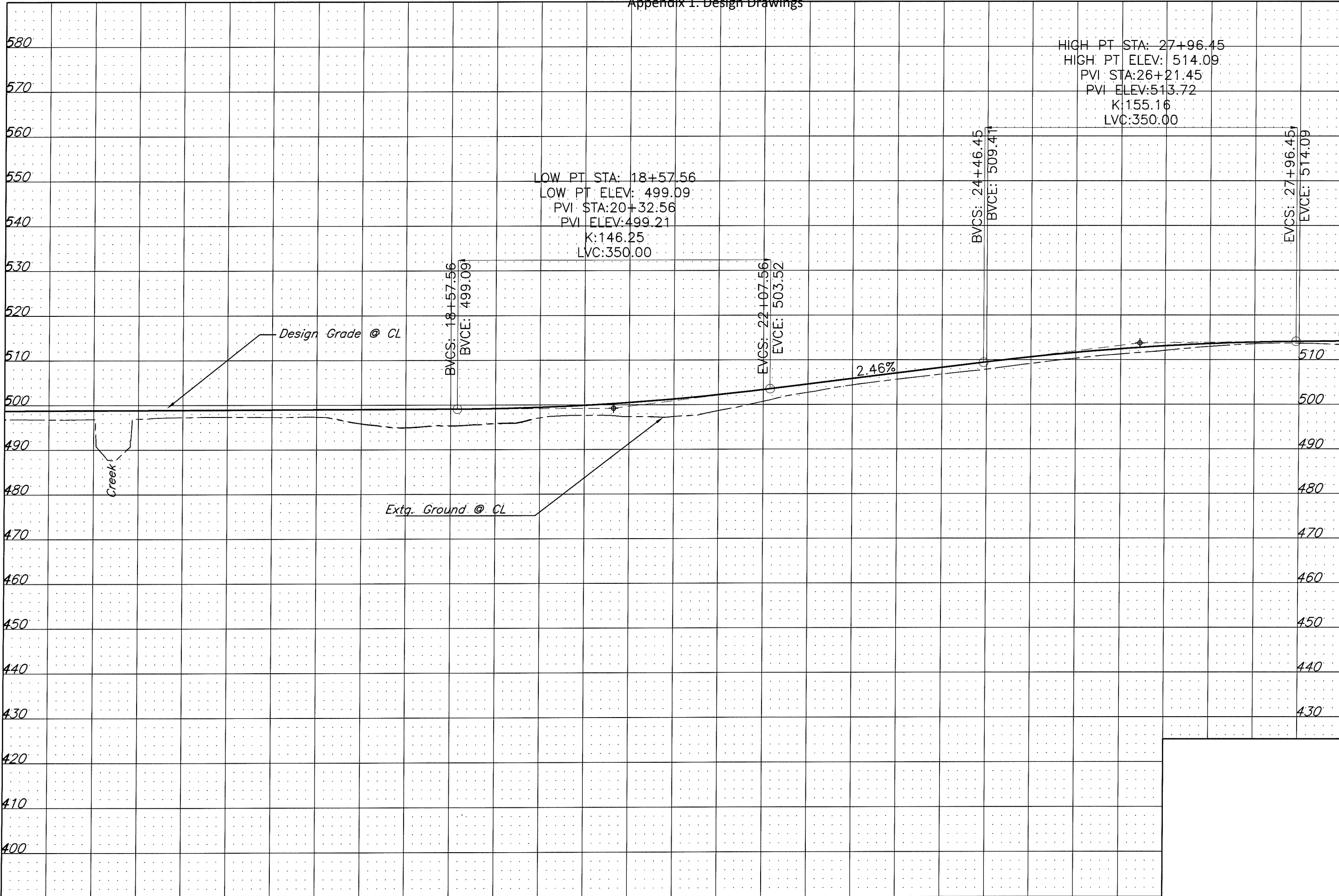
SHEET NO. 4

- ① Sta. L 22+91.64 Rt. To Sta. L 23+10.41 Rt.
 Const. Mailbox Turnout - 4' Wide
 (For Details, See Sht. 2B-x)
- ② Sta. L 23+29.21 Rt.
 Const. Asph. Conc. Drwy. - 12.6' Wide
 (For Details, See Dwg. RD715)
- ③ Sta. L 438+82.15, Lt. & Rt.
 Sawcut And Match Extg Pavement
- ④ Sta. L 14+72.15 Lt. & Rt.
 Raise Extg 32'x42' Bridge - x.x'
 Widen Extg 32'x42' Bridge - 1 Slabs Lt. & 4 Slab Rt.
 (For Details, See Sht. xx)
- ⑤ Sta. L 12+07.16 Rt. To Sta. L 14+53.80 Rt.
 Const. Guardrail, Type 2A - 190 Ft.
 Const. Guardrail, Type 3 - 25 Ft.
 Const. Guardrail Transition - 2 Each
 (For Details, See Dwg. RD400, RD405, RD415 and RD420)
- ⑥ Sta. L 12+02.10 Lt. To Sta. L 14+48.92 Lt.
 Const. Guardrail, Type 2A - 204 Ft.
 Const. Guardrail, Type 3 - 25 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
 Const. Guardrail Transition - 2 Each
- ⑦ Sta. L 14+93.81 Rt. To Sta. L 115+78.49 Rt.
 Const. Guardrail, Type 2A - 12.5 Ft.
 Const. Guardrail, Type 3 - 12.5 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
 W=1.0', E=2.0', L=50', Test Level 3
 Const. Guardrail Transition - 1 Each
- ⑧ Sta. L 14+91.26 Lt. To Sta. L 16+87.82 Lt.
 Const. Guardrail, Type 2A - 112.5 Ft.
 Const. Guardrail, Type 3 - 12.5 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
 W=1.0', E=2.0', L=50', Test Level 3
 Const. Guardrail Transition - 1 Each

Legend
 Cut Catch Line
 ----- Fill Catch Line



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

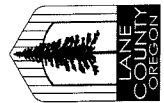


Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX

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LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Date	Revision	Appr'd

Filename: H:\Projects\17999-02\DESIGN\Plan&Profile\Chote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 5

Legend

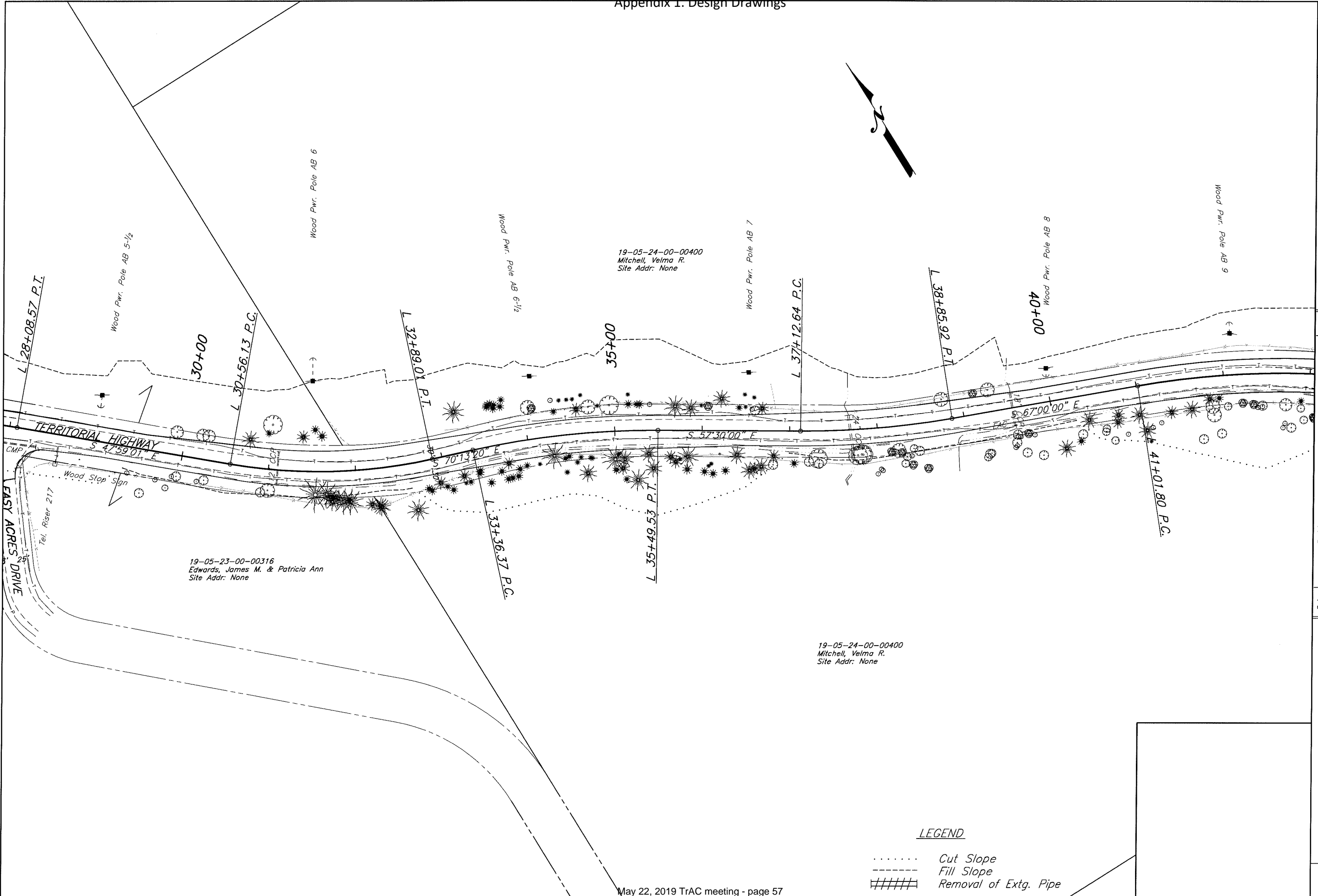
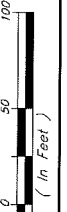
..... Cut Catch Line

----- Fill Catch Line



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director

WILLIAM F. MORGAN, P.E.
County Engineer

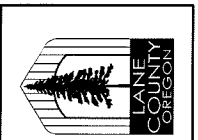
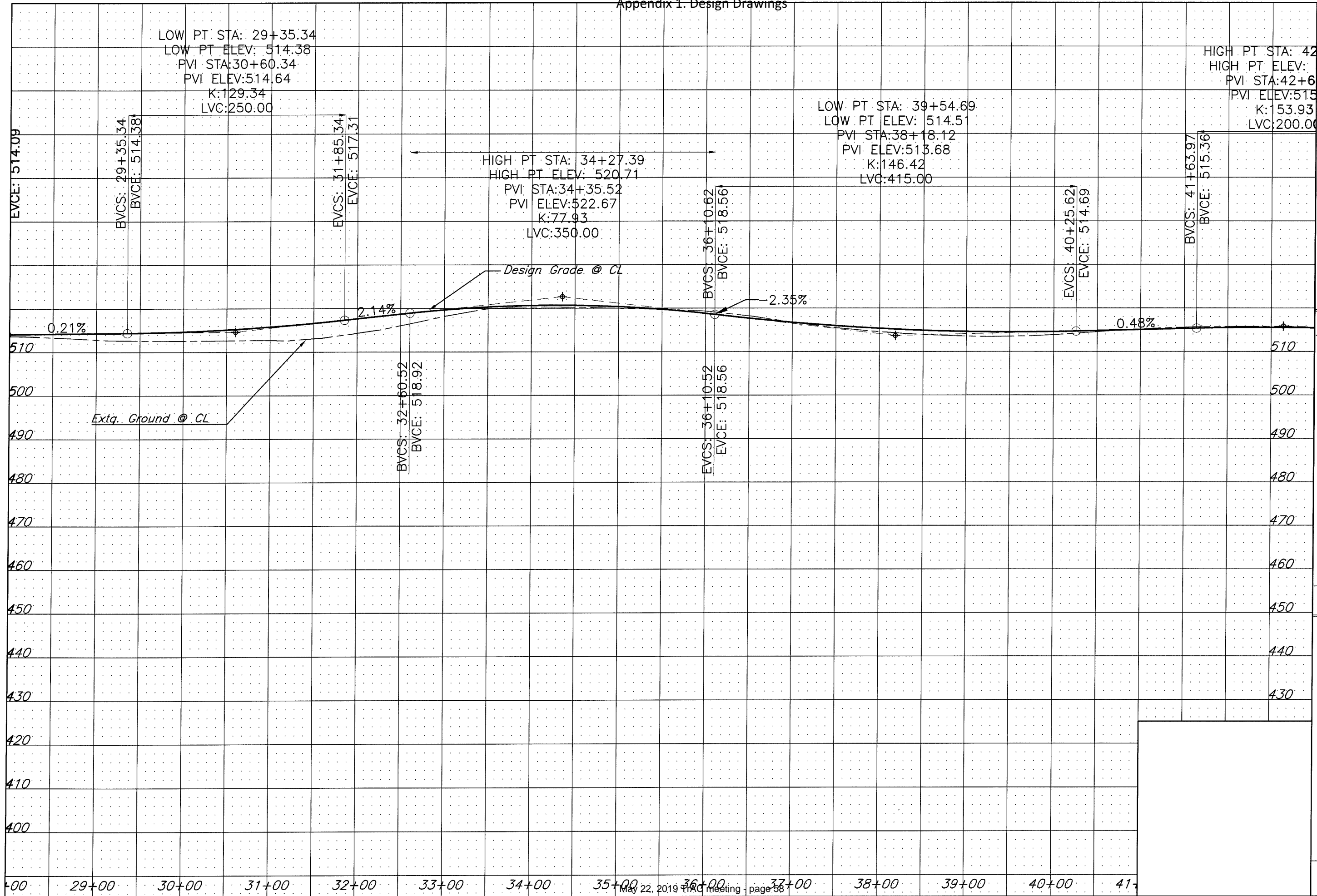


Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX) XXXXXX

SHEET NO. 5A

Appendix 1. Design Drawings



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX

SHEET NO. **5B**



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM E. MORGAN, P.E.
 County Engineer

Date	Revision	App'r'd

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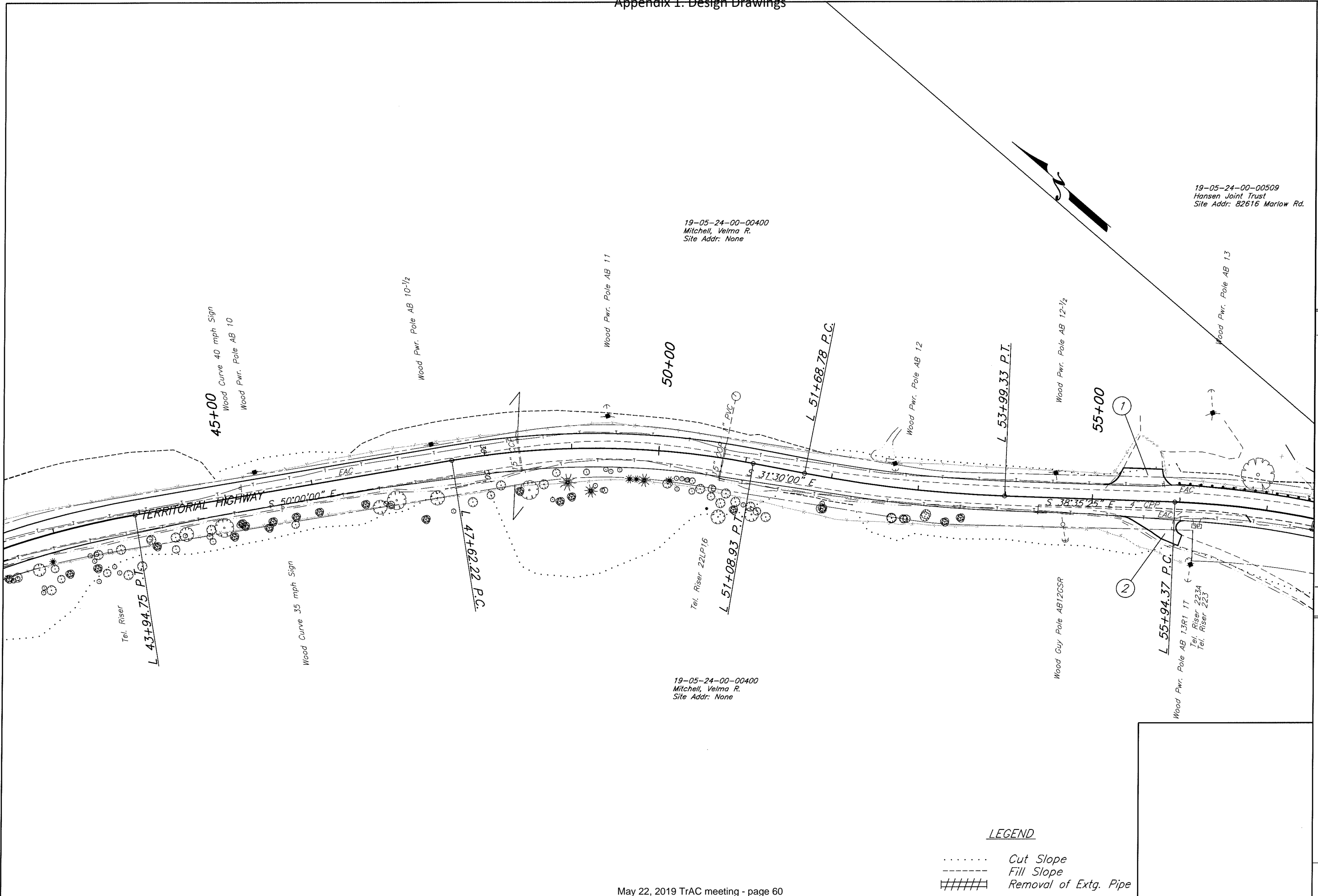
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE PROJECT NO. STATE HWY. NO.
 XXXXXX 201X STP-XXXX(XXX) XXXXXX

SHEET NO. 6

- ① Sta. L 55+52.26 Lt.
 Const. Asph. Conc. Drwy. - 44.6' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 55+71.51 Rt.
 Const. Asph. Conc. Drwy. - 12.7' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line



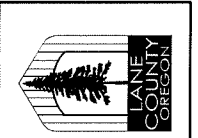
19-05-24-00-00400
Mitchell, Velma R.
Site Addr: None

19-05-24-00-00400
Mitchell, Velma R.
Site Addr: None

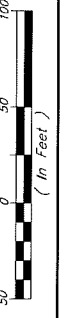
19-05-24-00-00509
Hansen Joint Trust
Site Addr: 82616 Marlow Rd.

LEGEND

- Cut Slope
- - - - - Fill Slope
- ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

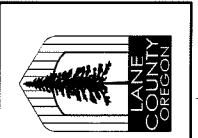
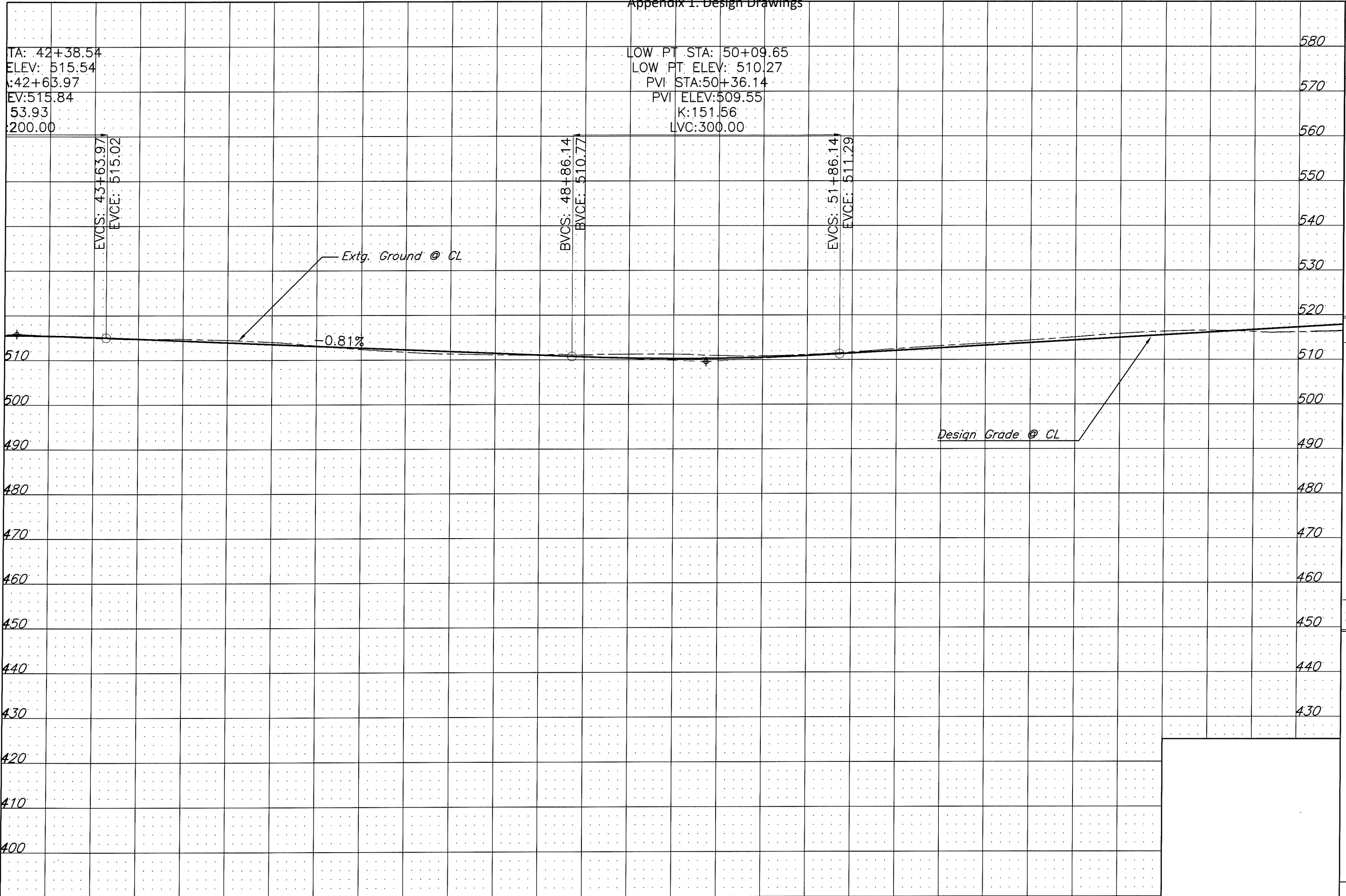


Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)
PROJECT NO.	XXXXXX

SHEET NO. **6A**

Appendix 1. Design Drawings



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

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XXXXXX 201X	STP-XXXX(XXX)	XXXXXX



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN P.E.
 County Engineer

Date	Revision	Appr'd

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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

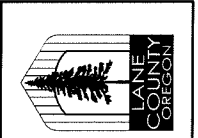
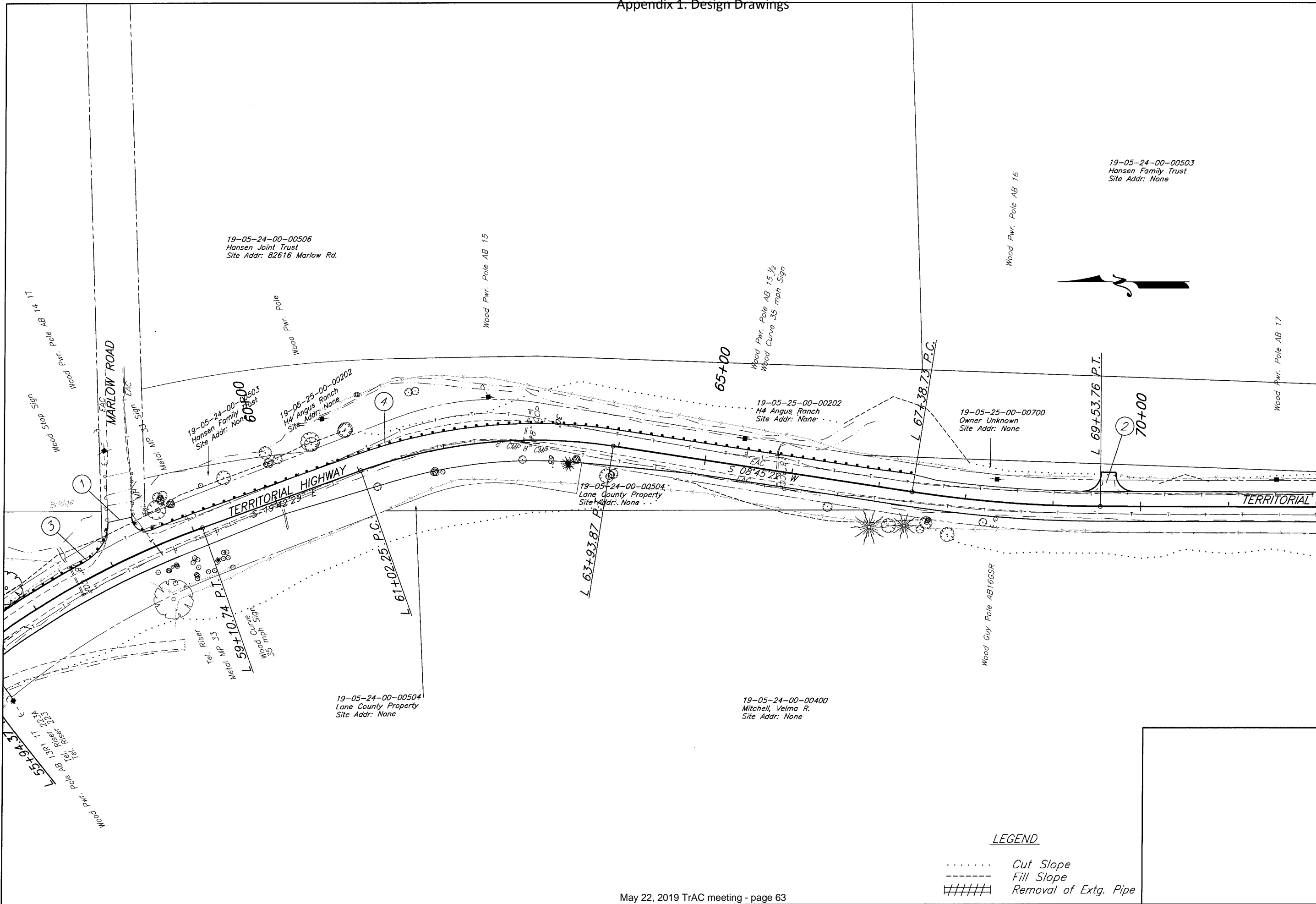
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	SIP-XXXX(XXX)	XXXXXX

SHEET NO. 7

- ① Sta. L 58+28.96, 44.18 Lt.
Sawcut And Match Extg Pavement
- ② Sta. L 69+58.65 Lt.
Const. Asph. Conc. Drwy. - 17.2' Wide
(For Details, See Dwg. RD715)
- ③ Sta. L 55+95.55 Lt. To Sta. L 58+17.69 Lt.
Const. Guardrail, Type 2A - 187.5 Ft.
Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
W=1.0', E=2.0', L=50', Test Level 3
(For Details, See Dwg. RD400, RD405, RD415 and RD420)
- ④ Sta. L 58+43.33 Lt. To Sta. L 67+34.39 Lt.
Const. Guardrail, Type 2A - 875 Ft.
Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 1 Each
W=1.0', E=2.0', L=50', Test Level 3

Legend

- Cut Catch Line
- Fill Catch Line



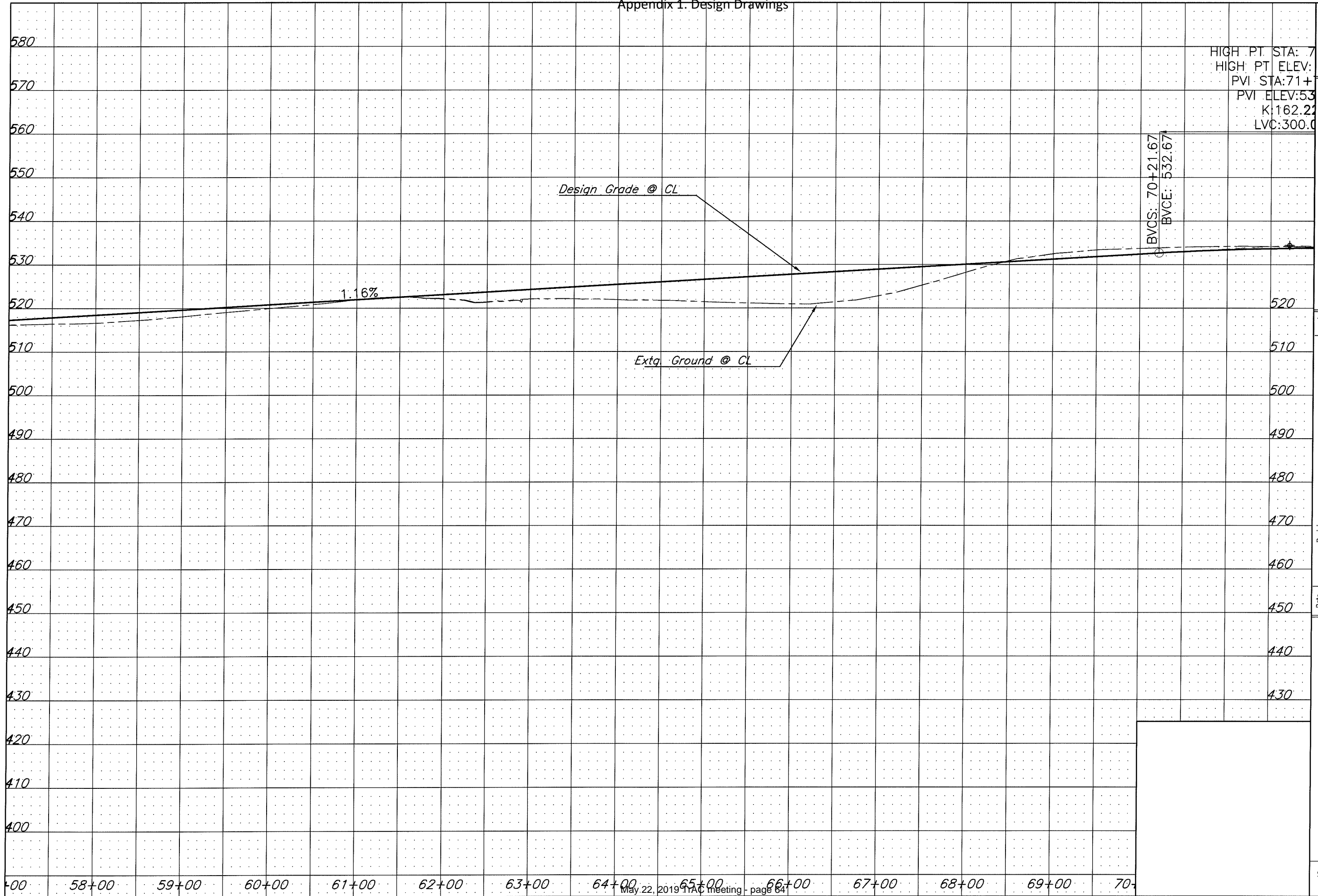
LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

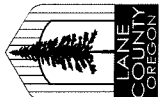
GRAPHIC SCALE
 0 50 100
 (In Feet)

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	PROJECT NO.
XXXXXX 201X	STP-XXXX(XXX)
STATE HWY. NO.	XXXXXX
SHEET NO.	7A

Appendix 1. Design Drawings





LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director

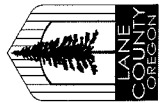
WILLIAM F. MORGAN P.E.
County Engineer

Date	Revision	Appr'd

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DATE XXXXXX 201X	PROJECT NO. STP-XXXX(XXX)
STATE HWY. NO. XXXXXX	XXXXXX

SHEET NO. 7B



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer

Date	Revision	Appr'd

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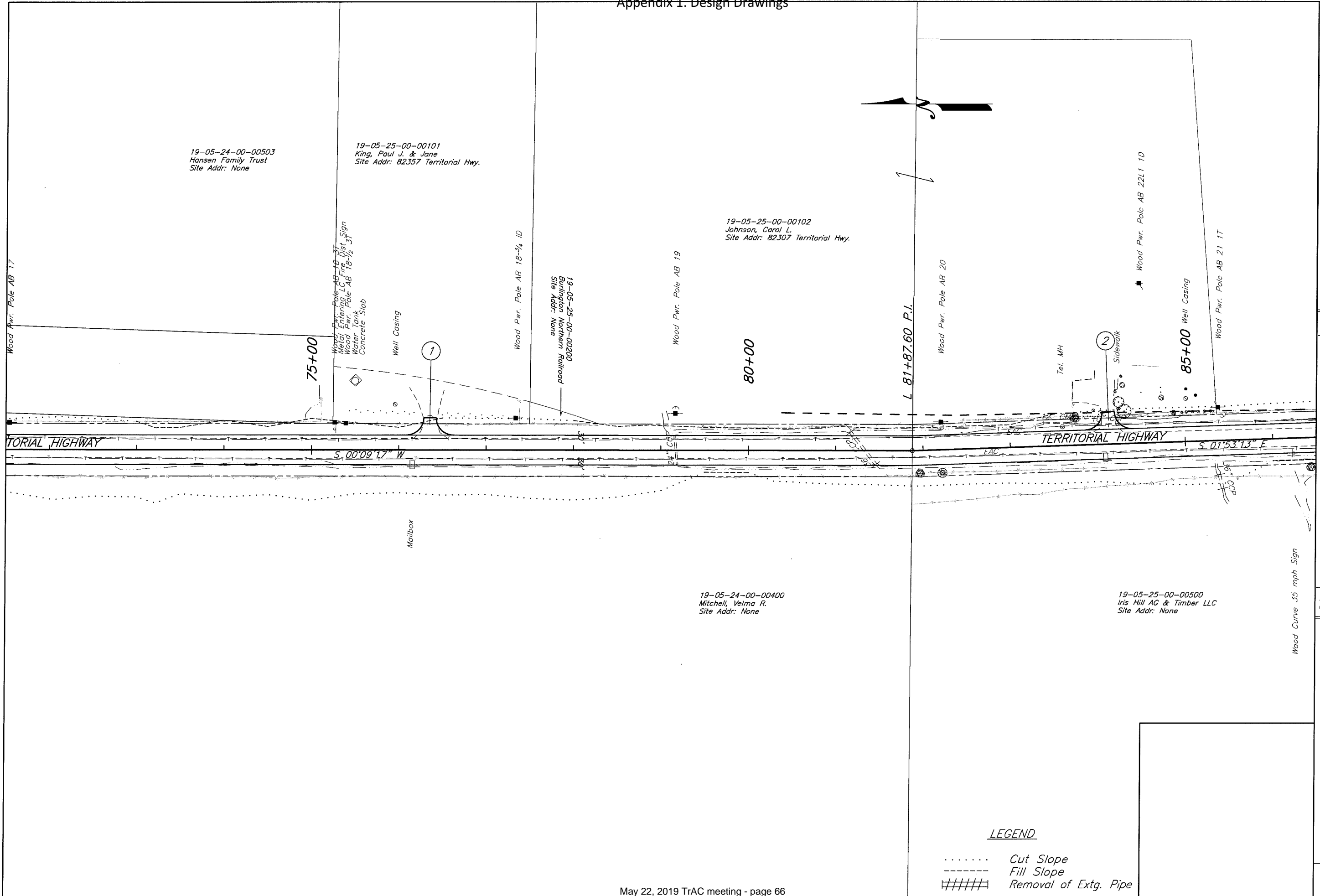
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE PROJECT NO. STATE HWY. NO.
 XXXXXX 201X STP-XXXX(XXX) XXXXXX

SHEET NO. 8

- ① Sta. L 76+31.18 Lt.
 Const. Asph. Conc. Drwy. - 14.0' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 84+08.61 Lt.
 Const. Asph. Conc. Drwy. - 14' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line



19-05-24-00-00503
Hansen Family Trust
Site Addr: None

19-05-25-00-00101
King, Paul J. & Jane
Site Addr: 82357 Territorial Hwy.

19-05-25-00-00102
Johnson, Carol L.
Site Addr: 82307 Territorial Hwy.

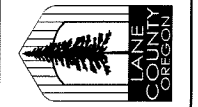
19-05-25-00-00200
Burlington Northern Railroad
Site Addr: None

19-05-24-00-00400
Mitchell, Velma R.
Site Addr: None

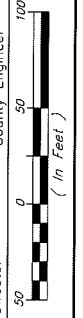
19-05-25-00-00500
Iris Hill AG & Timber LLC
Site Addr: None

LEGEND

- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer



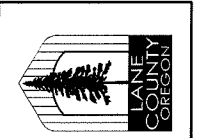
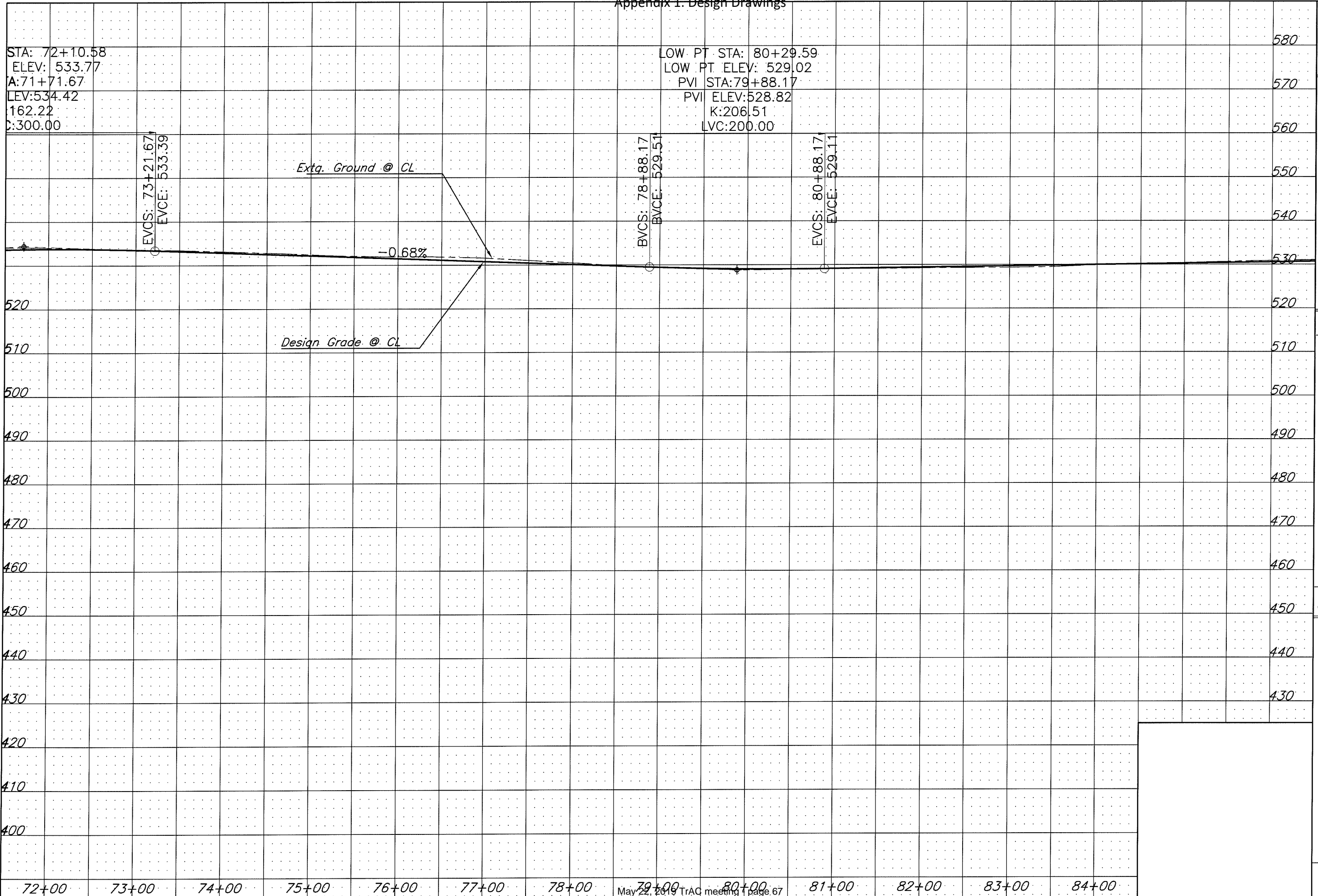
Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX

SHEET NO. 8A

Appendix 1. Design Drawings

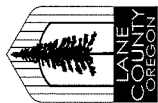


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 WILLIAM F. MORGAN, P.E.
 County Engineer
 MARSHA A. MILLER
 Public Works Director

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

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 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer

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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

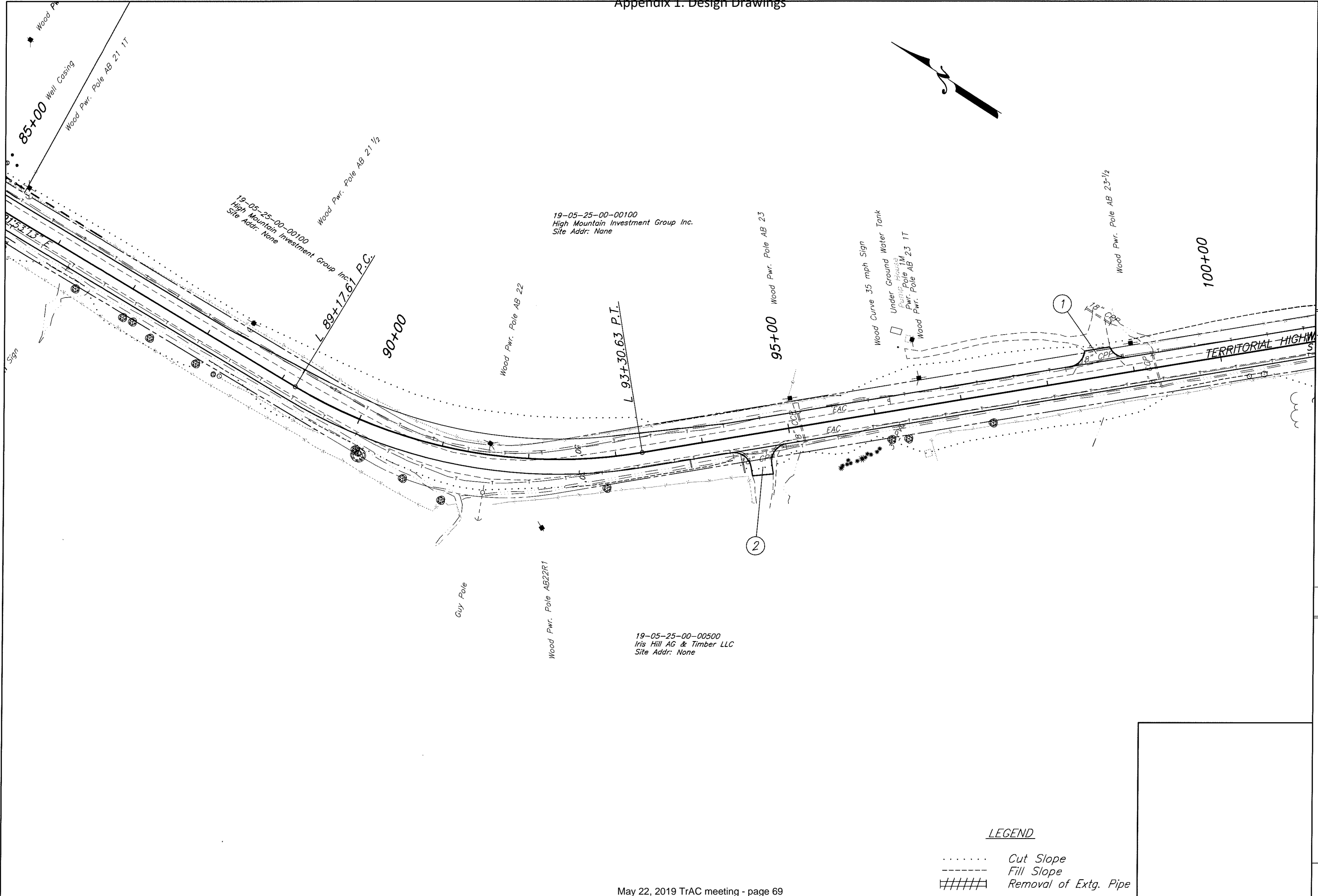
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 9

- ① Sta. L 98+58.04 Lt.
 Const. Asph. Conc. Drwy. - 28.7' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 94+58.37 Rt.
 Const. Asph. Conc. Drwy. - 24.1' Wide

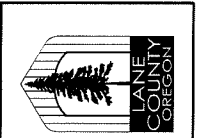
Legend

- Cut Catch Line
- Fill Catch Line

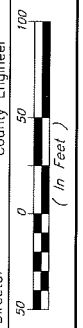


LEGEND

.....	Cut Slope
-----	Fill Slope
#####	Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer



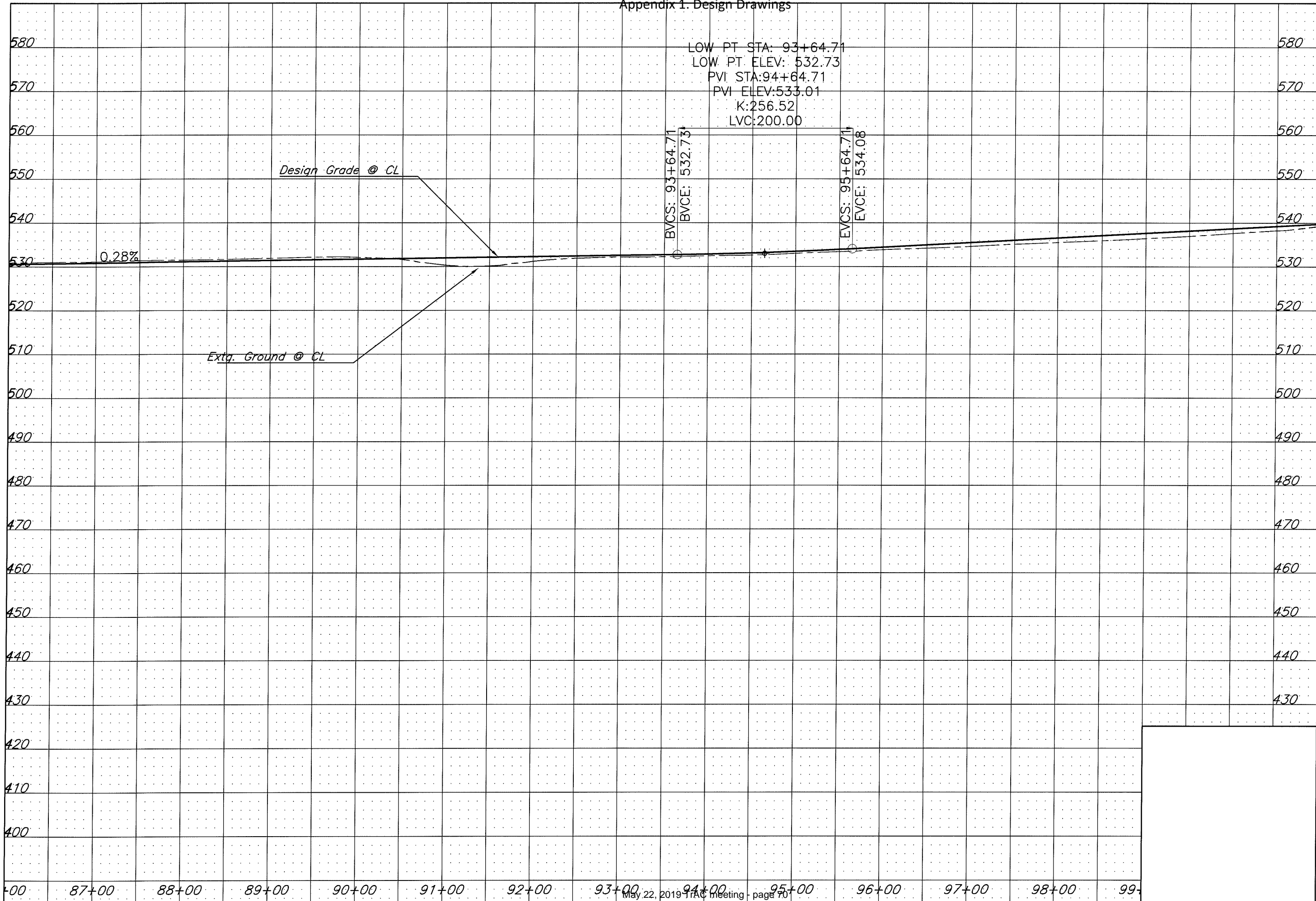
Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

Filename: H:\Projects\147989-02\DES\PlanSet\Plan&Profile\Topog.dwg

Appendix 1. Design Drawings



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Appr'd	Revision	Date

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 PROJECT NO. STP-XXXX(XXX)
 STATE HWY. NO. XXXXXX
 DATE XXXXXX 201X



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

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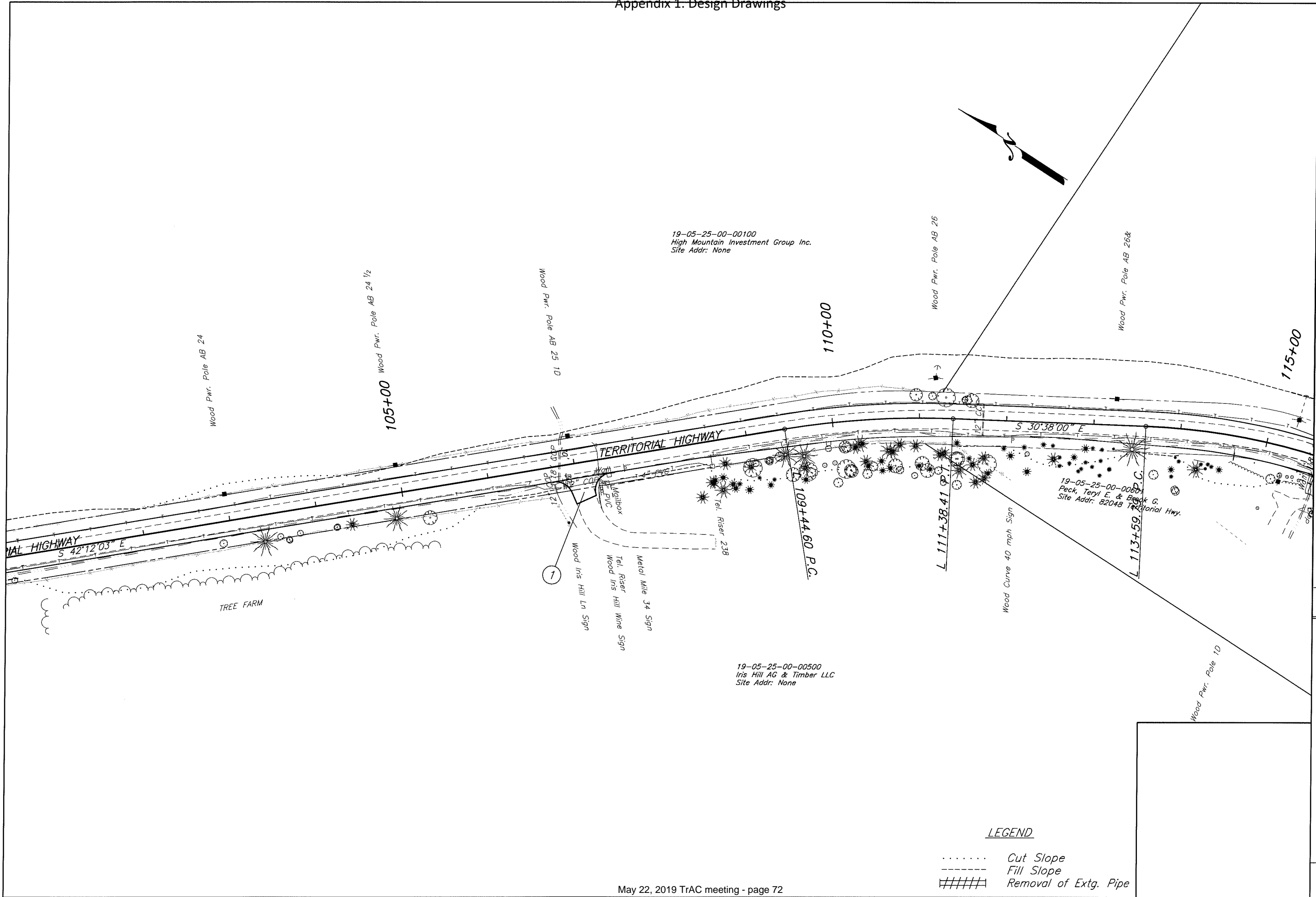
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 10

① Sta. L 107+03.64 Rt.
 Const. Asph. Conc. Drwy. - 23.9' Wide
 (For Details, See Dwg. RD715)

Legend
 Cut Catch Line
 ----- Fill Catch Line



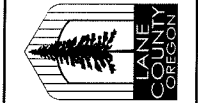
19-05-25-00-00100
High Mountain Investment Group Inc.
Site Addr: None

19-05-25-00-00600
Peck, Teryl E. & Brock G.
Site Addr: 82048 Territorial Hwy.

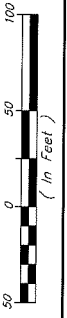
19-05-25-00-00500
Iris Hill AG & Timber LLC
Site Addr: None

LEGEND

- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN P.E.
County Engineer



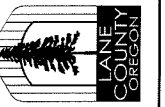
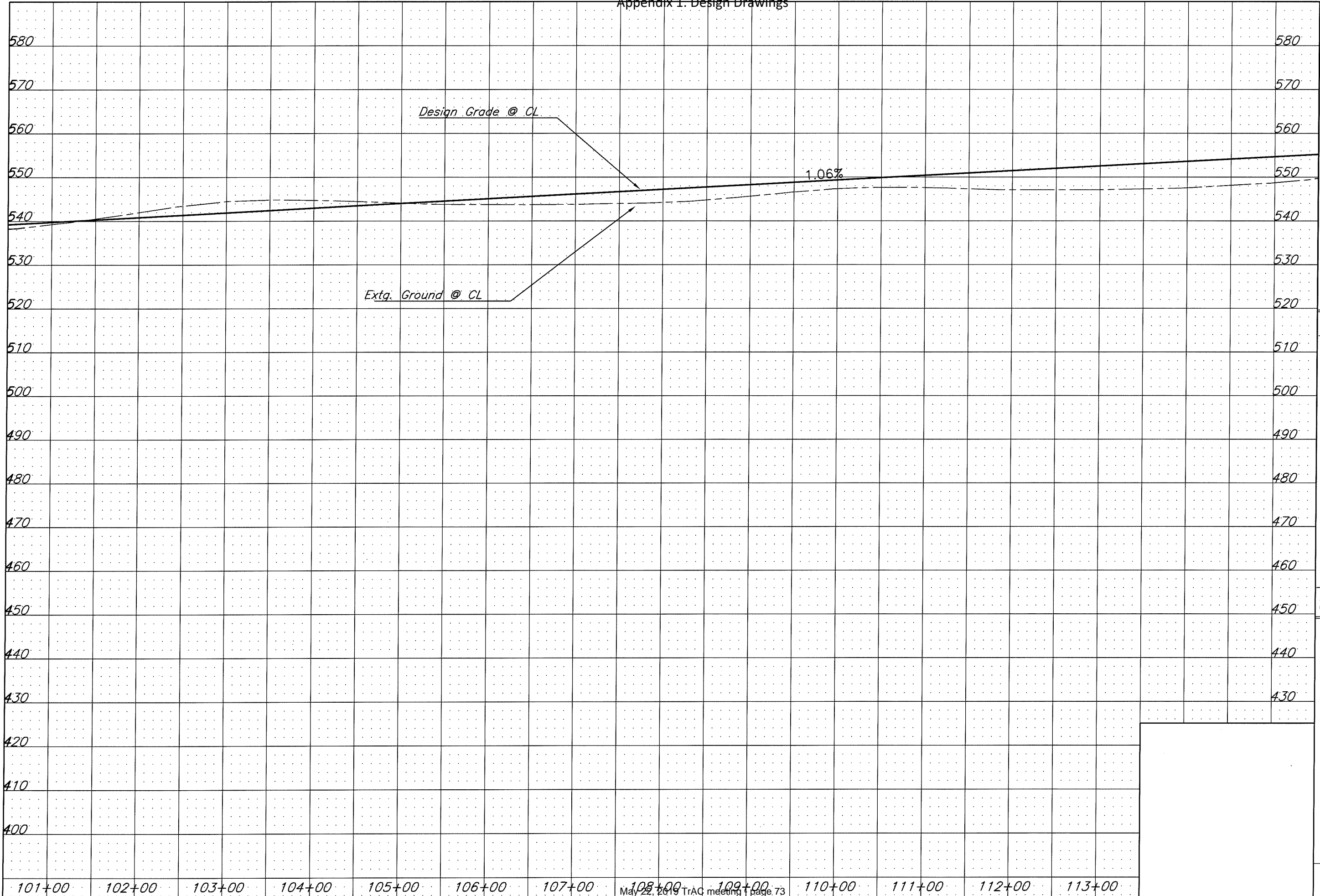
Revision	Date	Appr'd

Filename: \\Projects\77889-02\DES\Plan&Profile\Topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	XXXXXX 201X		

SHEET NO. **10A**

Appendix 1. Design Drawings

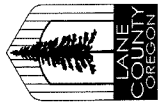


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Appr'd	Revision	Date

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXX

SHEET NO. 10B



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN, P.E.
 County Engineer

Revision	Date	App'r'd

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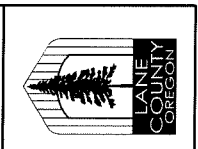
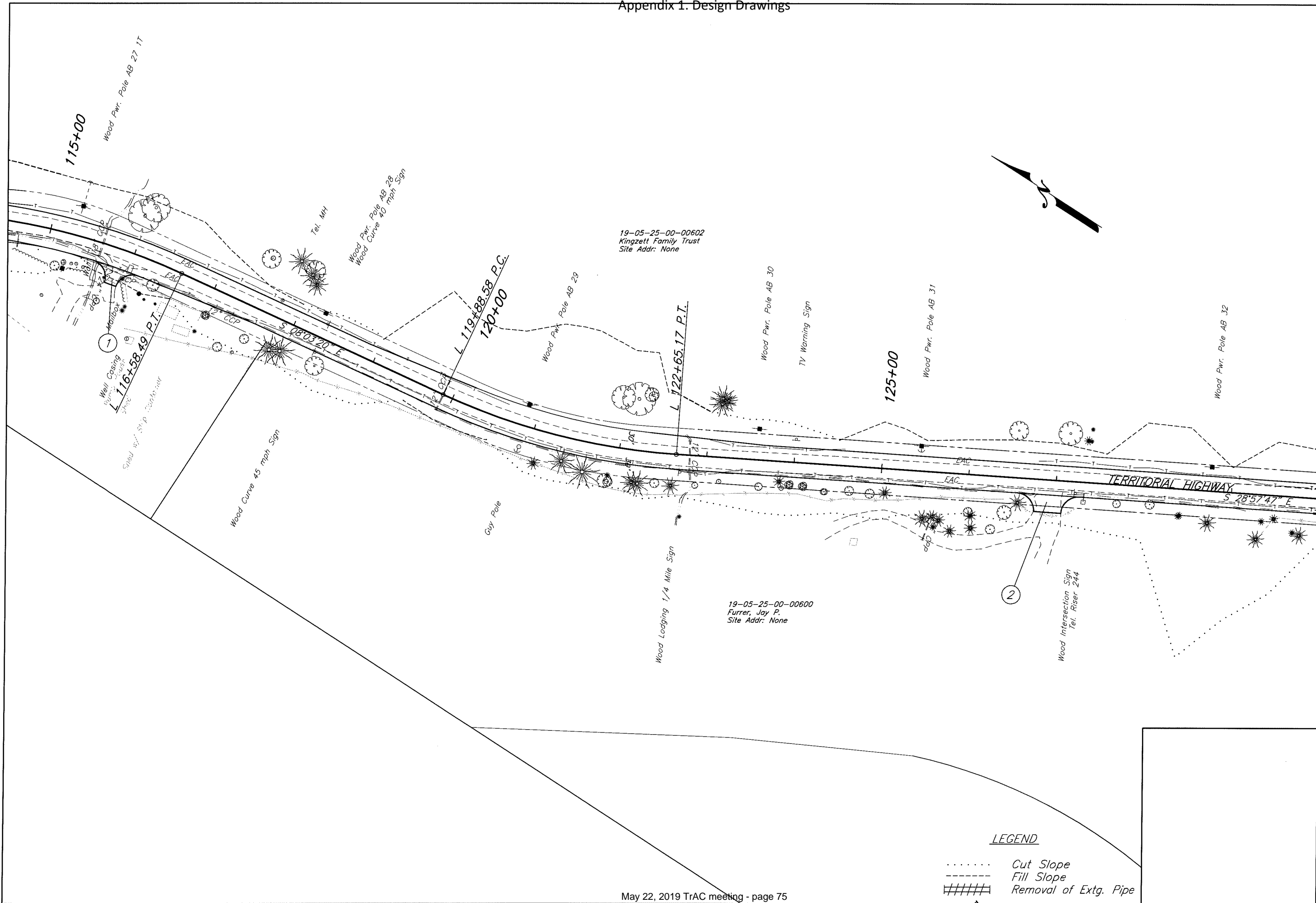
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 11

- ① Sta. L 115+81.02 Rt.
 Const. Asph. Conc. Drwy. - 14.2' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 126+87.77 Rt.
 Const. Asph. Conc. Drwy. - 31.1' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

GRAPHIC SCALE
0 50 100
(In Feet)

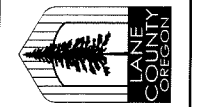
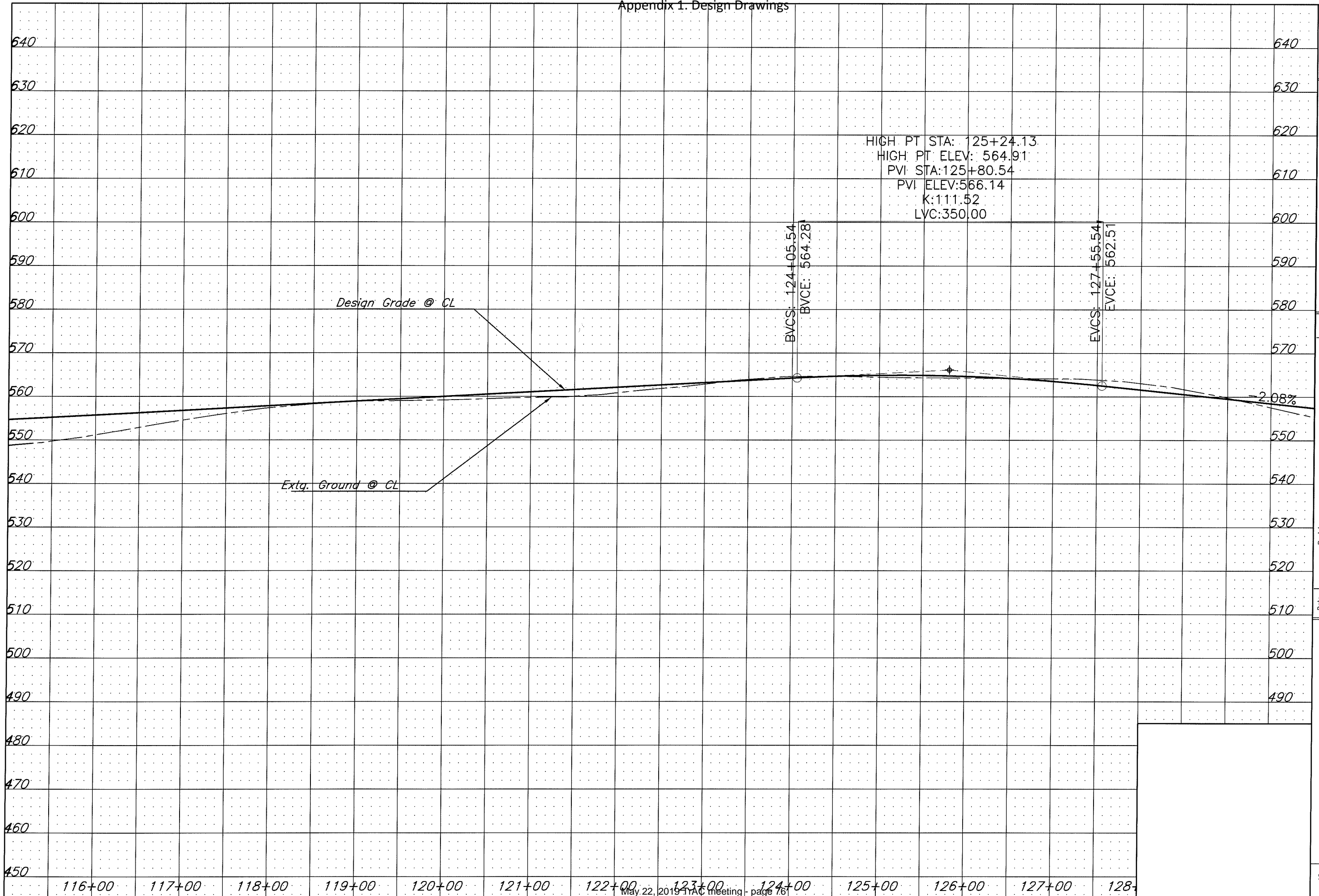
Date	Revision	Appr'd

Filename: H:\Projects\A7989-02\DCS\Planest\Final\Profile_Topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 11A

Appendix 1. Design Drawings



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
WILLIAM F. MORGAN, P.E.
County Engineer

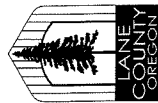
MARSHA A. MILLER
Public Works Director

Revision	Date	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
PROJECT NO: STP-XXXX(XXX)
STATE HWY. NO: XXXXXX

SHEET NO. 11B



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director

WILLIAM F. MORGAN, P.E.
County Engineer

1

Sta. LH 1+27.49, Lt. & Rt.
Sawcut And Match Extg Pavement

Revision	Date	Appr'd

Filename: H:\Projects\#A-7989-02\UES\Planset\Plan&Profile\Glete.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

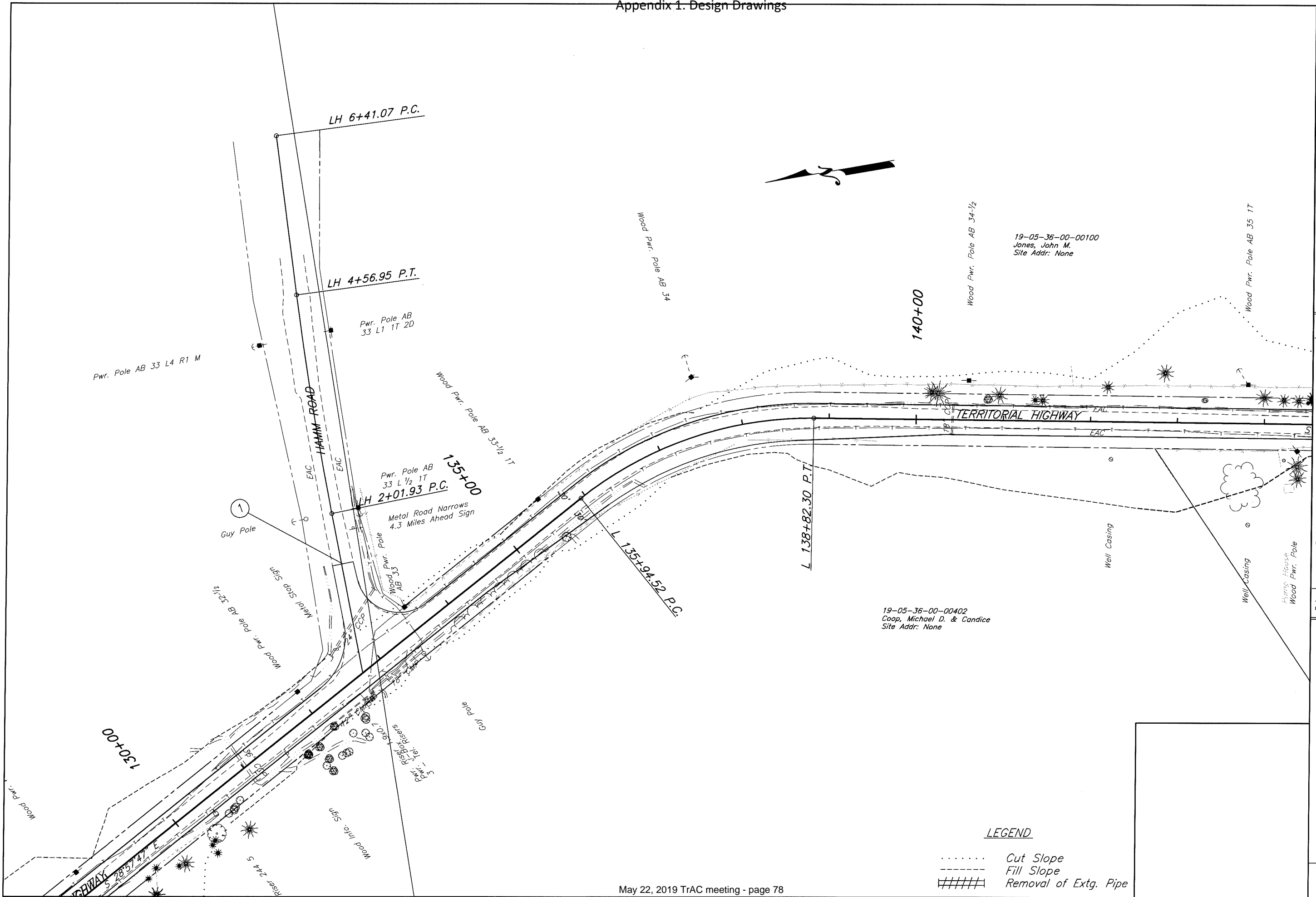
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 12

Legend

..... Cut Catch Line

----- Fill Catch Line



LEGEND

- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MABSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

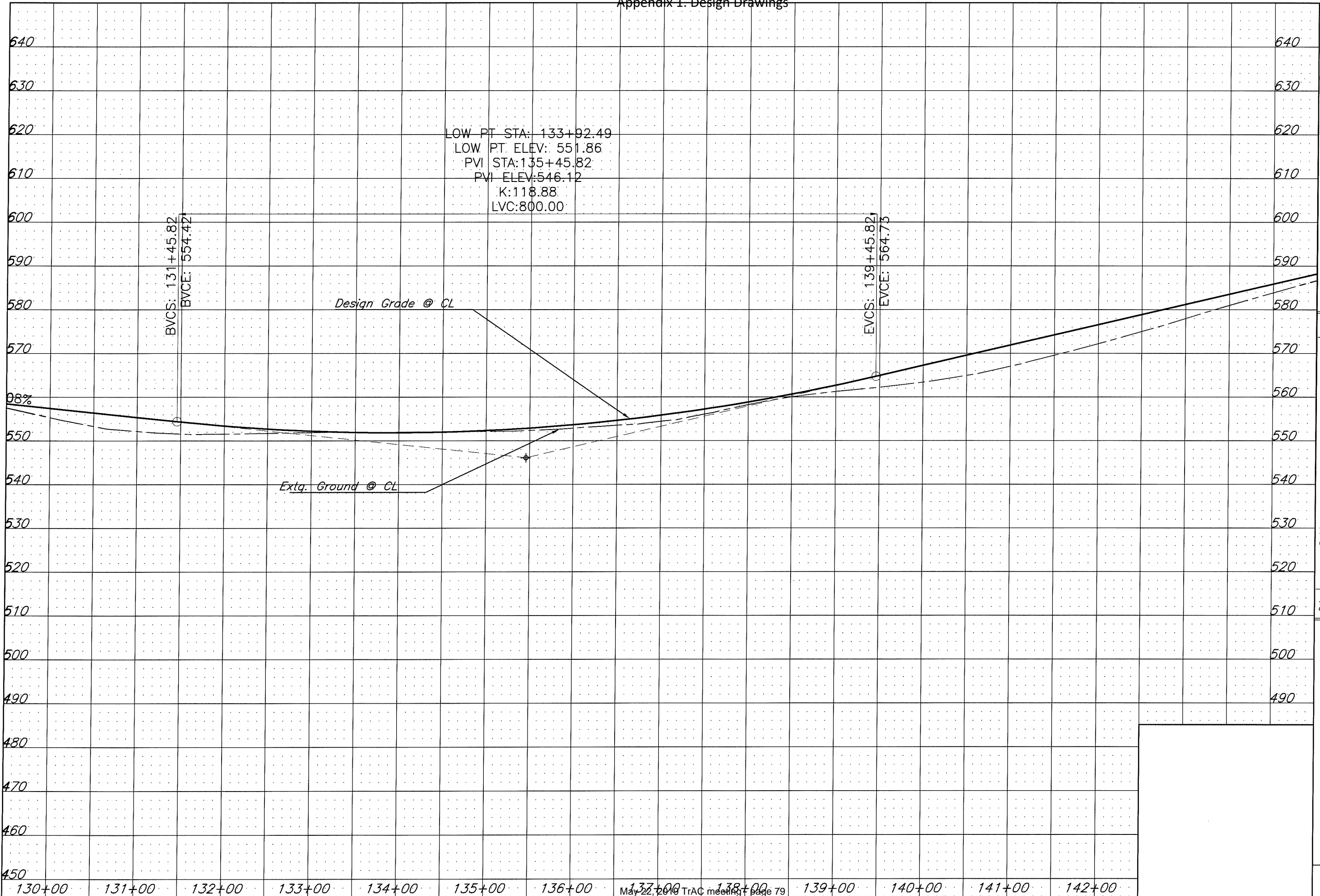


Appr'd	Date	Revision

Filename: H:\Projects\A7989-02\DES\Plan&Profile\Topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
XXXXXXX 201X	STP-XXXX(XXX) XXXXX

SHEET NO. **12A**

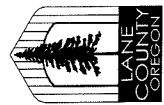


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

Filename: H:\Projects\#7989-02\DES\Plan&Profile\Plan&Profile\CHote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

STATE HWY. NO.
 XXXXXX

PROJECT NO.
 STP-XXXX(XXX)

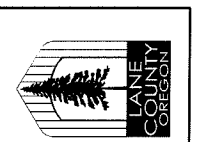
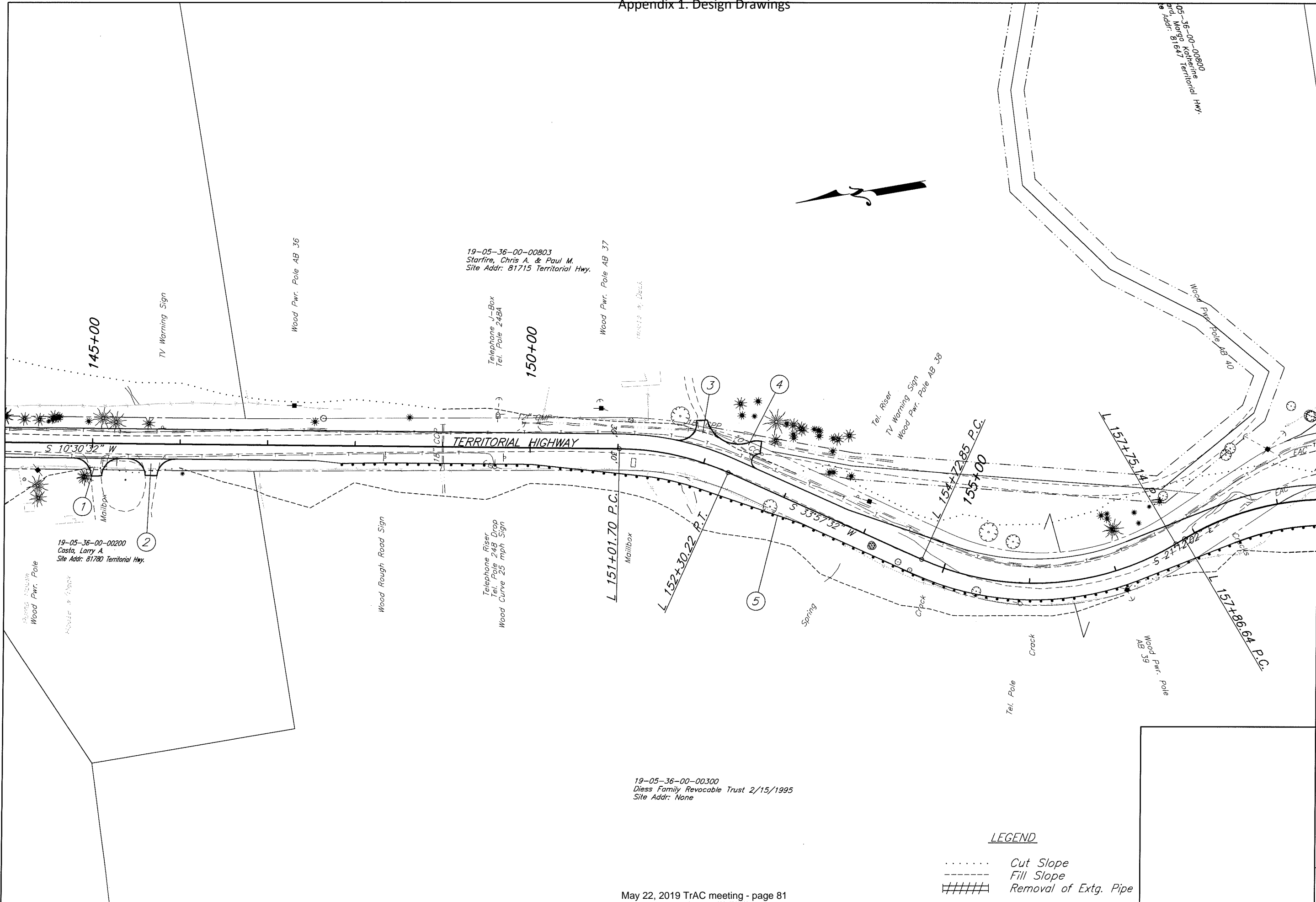
DATE
 XXXXXX, 201X

SHEET NO. 13

- ① Sta. L 145+00.62 Rt.
 Const. Asph. Conc. Drwy. - 12' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 145+63.32 Rt.
 Const. Asph. Conc. Drwy. - 13.3' Wide
- ③ Sta. L 151+90.81 Lt.
 Const. Asph. Conc. Drwy. - 12' Wide
- ④ Sta. L 152+32.95 Lt.
 Const. Asph. Conc. Drwy. - 14' Wide
- ⑤ Sta. L 147+80.97 Rt. To Sta. L 161+44.07 Rt.
 Const. Guardrail, Type 2A - 1237.5 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 2 Each
 W=1.0', E=2.0', L=50', Test Level 3
 (For Details, See Dwg. RD400, RD405, RD415 and RD420)

Legend

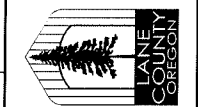
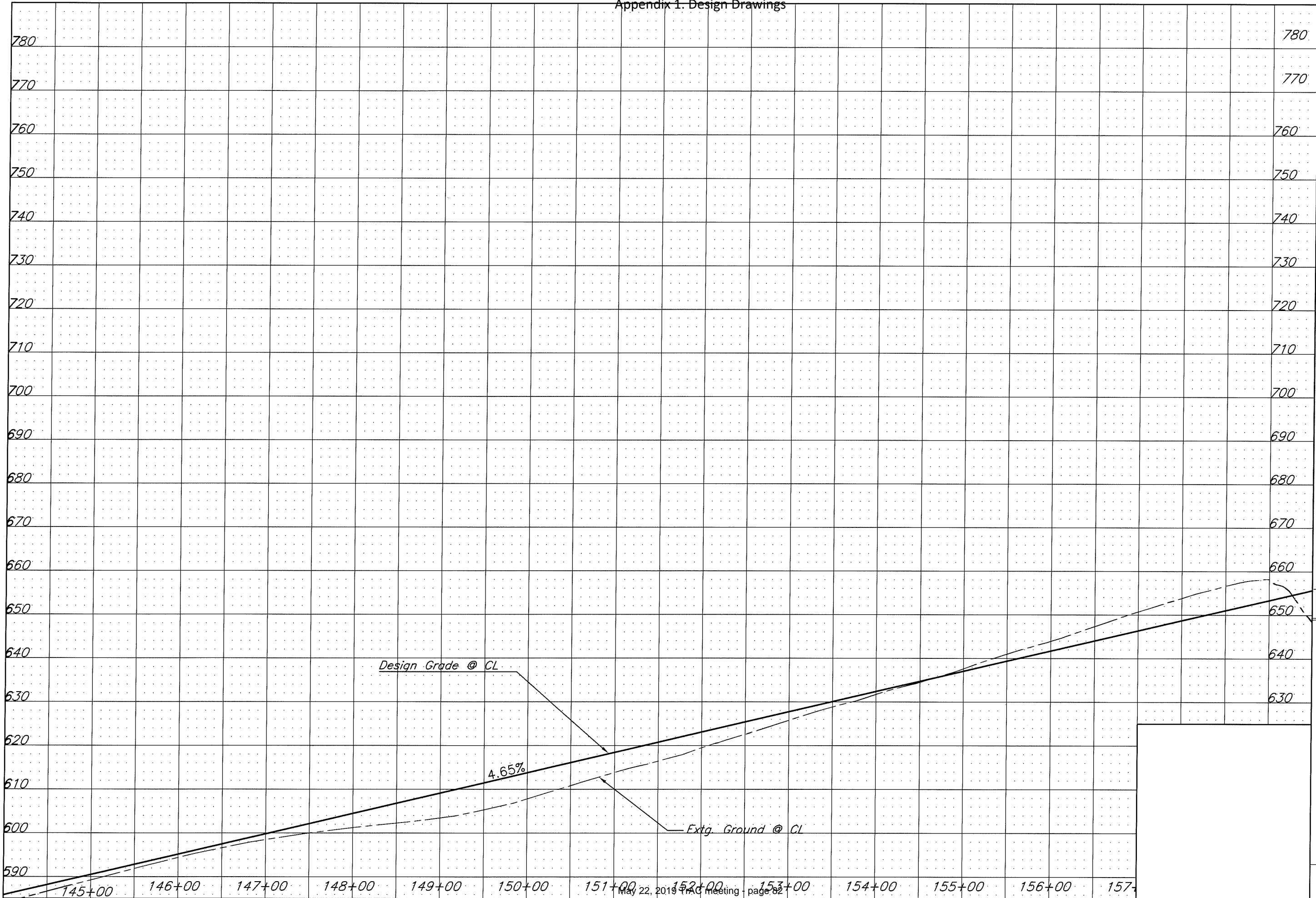
- Cut Catch Line
- Fill Catch Line



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer
 GRAPHIC SCALE
 0 50 100
 (in Feet)

Appr'd	Revision	Date

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 DATE: 201X
 PROJECT NO: STIP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX
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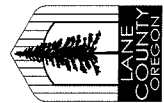
LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM E. MORGAN, P.E.
 County Engineer

Appr'd	Revision	Date

GILLESPIE CORNERS TO COTTAGE GROVE-LOKANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

Filename: H:\Projects\17289-02\DES\PlanSet\Plan&Profile\Topog.dwg



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer

Date	Revision	Appr'd

Filename: H:\Projects\747989-02\DES\PlanSet\Plan&Profile\Chote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

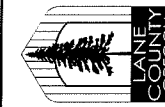
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 PROJECT NO.: STP-XXX(XXX)
 STATE HWY. NO.: XXXXXX

SHEET NO. 14

- ① Sta. L 161+96.24 Lt.
 Const. Asph. Conc. Drwy. - 18.5' Wide
 (For Details, See Dwg. RD715)
- ② Sta. L 164+81.67 Lt. To Sta. L 161+44.07 Lt.
 Const. Guardrail, Type 2A - 262.5 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 2 Each
 W=1.0', E=2.0', L=50', Test Level 3
 (For Details, See Dwg. RD400, RD405, RD415 and RD420)
- ③ Sta. L 147+80.97 Rt. To Sta. L 161+44.07 Rt.
 Const. Guardrail, Type 2A - 800 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 2 Each
 W=1.0', E=2.0', L=50', Test Level 3

Legend

- Cut Catch Line
- Fill Catch Line



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

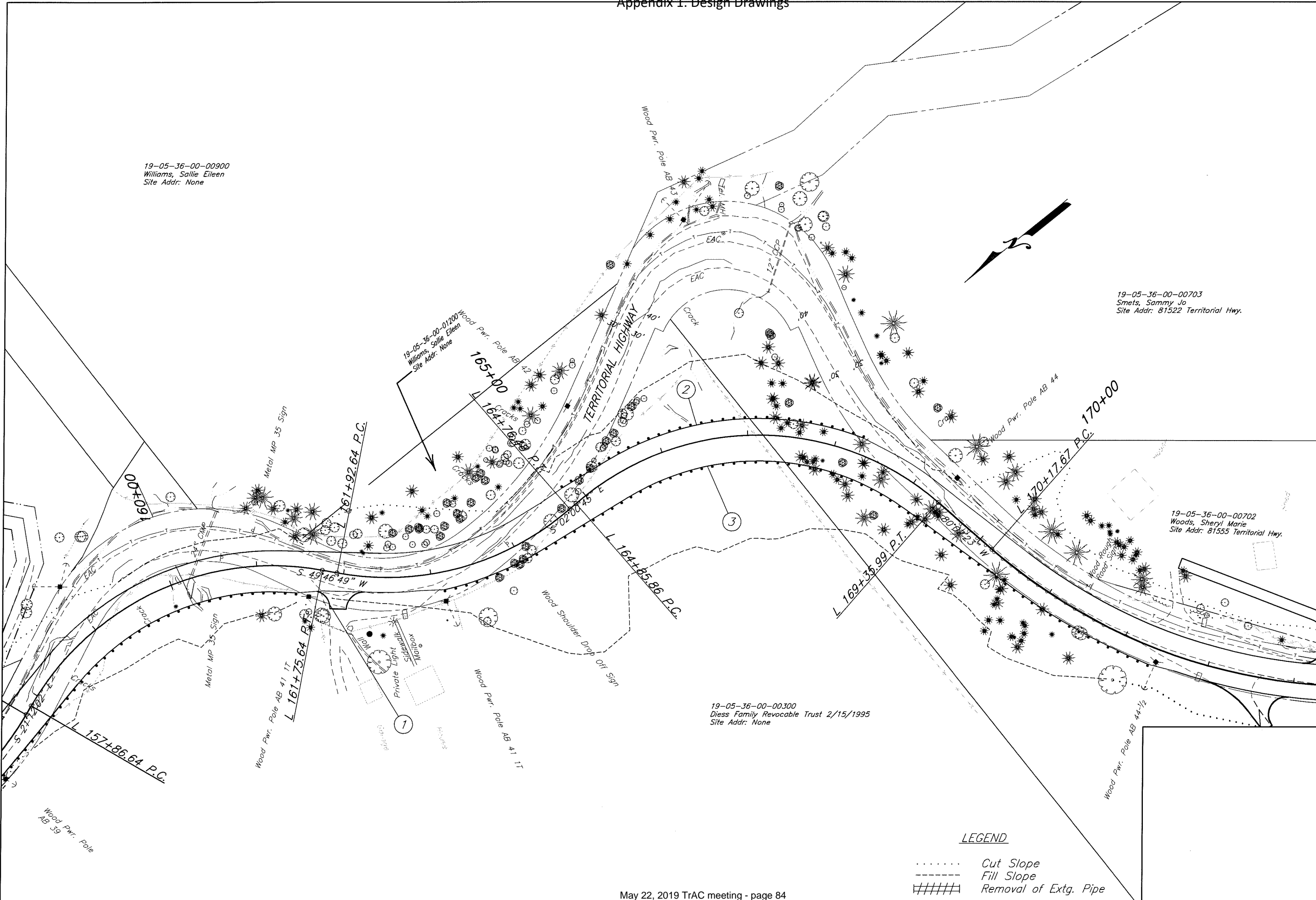


19-05-36-00-00900
 Williams, Sallie Eileen
 Site Addr: None

19-05-36-00-00703
 Smets, Sammy Jo
 Site Addr: 81522 Territorial Hwy.

19-05-36-00-00702
 Woods, Sheryl Marie
 Site Addr: 81555 Territorial Hwy.

19-05-36-00-00300
 Diess Family Revocable Trust 2/15/1995
 Site Addr: None



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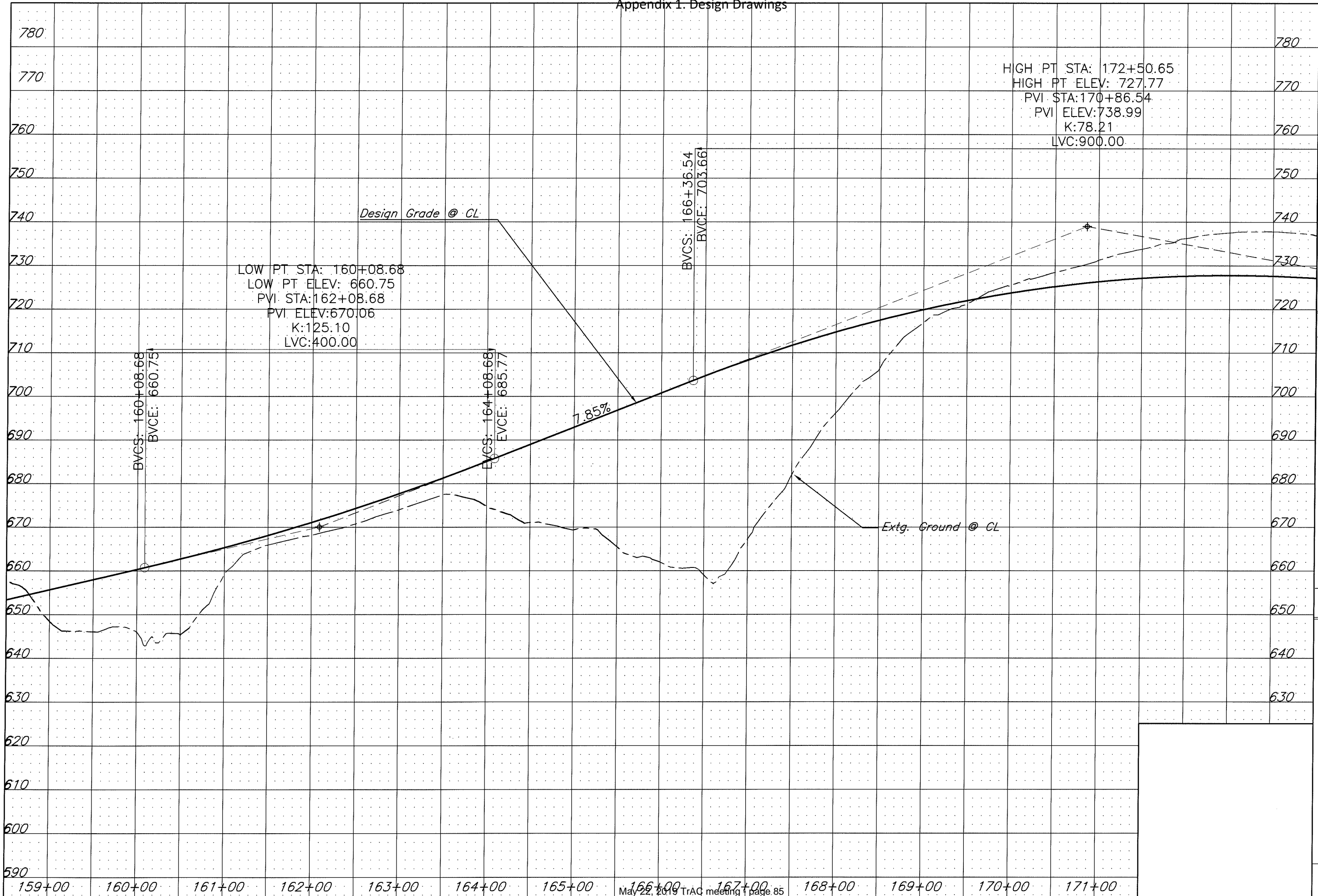
- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe


Appr'd	Date	Revision

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
PROJECT NO.	STATE HWY. NO.
STP-XXXX(XXX)	XXXXXX
DATE	
XXXXXX 201X	

SHEET NO. 14A

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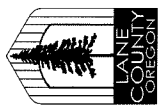
LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

Filename: U:\Projects\170989-02\DES\Profile\Plan&Profile_Topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE XXXXXX 201X	STATE HWY. NO. XXXXXX
PROJECT NO. STP-XXXX(XXX)	

SHEET NO. **14B**



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

Filename: H:\Projects\#7989-02\DES\Plan&Profile\Plan&Profile\CHote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

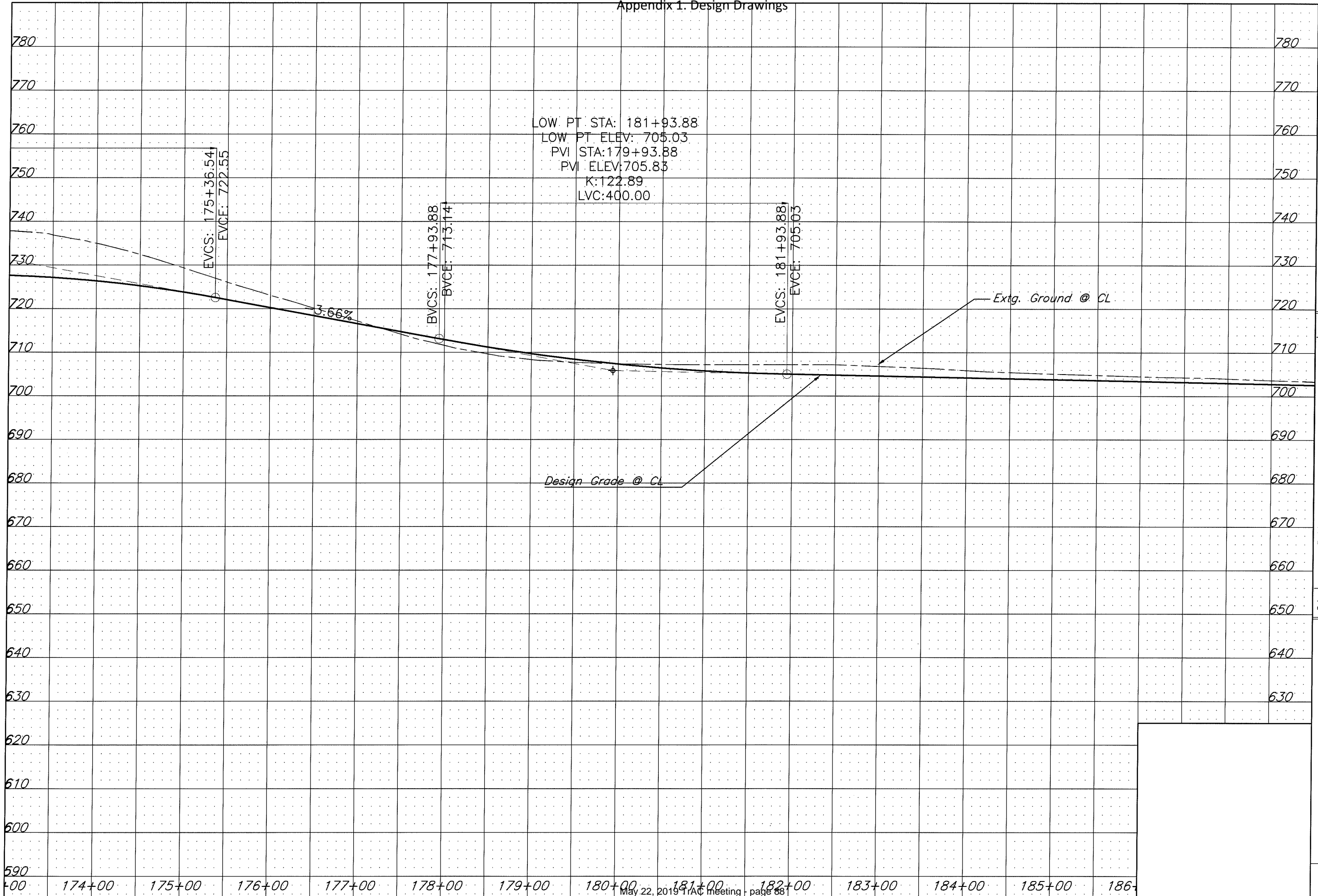
DATE: XXXXXX, 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

SHEET NO. 15

- ① Sta. L 176+67.58 Lt.
Const. Asph. Conc. Drwy. - 28' Wide
(For Details, See Dwg. RD715)
- ② Sta. L 182+76.72 Lt.
Const. Asph. Conc. Drwy. - 16' Wide
- ③ Sta. L 182+73.04 Rt.
Const. Asph. Conc. Drwy. - 23' Wide
- ④ Sta. L 173+58.83 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ⑤ Sta. L 174+35.90 Lt.
Const. Asph. Conc. Drwy. - 12' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line

Appendix 1. Design Drawings

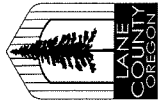


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: XXXXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSIA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

①

Sta. L 193+54.49 Lt.
Const. Asph. Conc. Drwy. - 12.7' Wide
(For Details, See Dwg. RD715)

Date	Revision	Appr'd

Filename: H:\Projects\#7989-02\DES\Planaset\Plan&Profile\CHote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

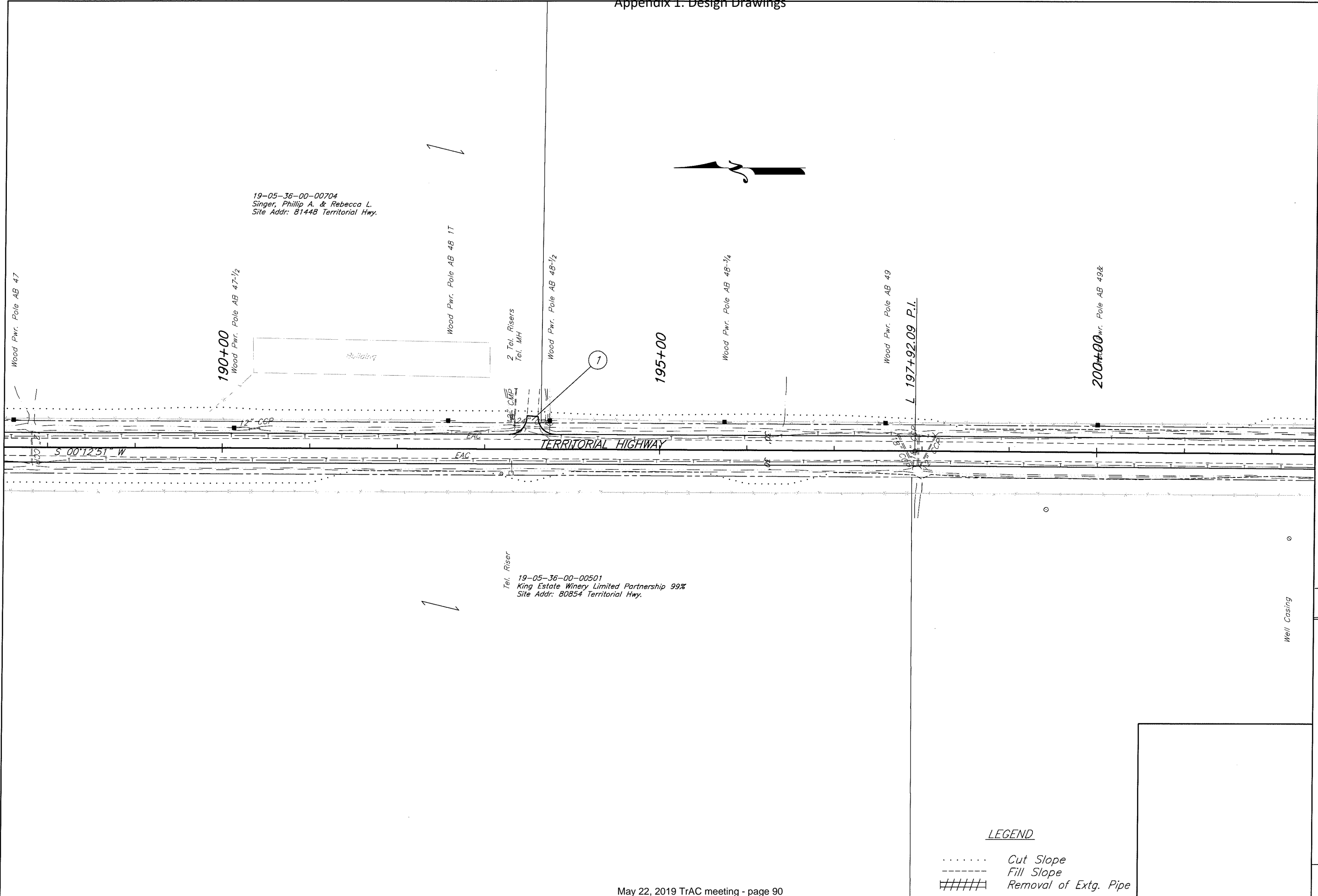
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 16

Legend

..... Cut Catch Line

----- Fill Catch Line



19-05-36-00-00704
 Singer, Phillip A. & Rebecca L.
 Site Addr: 81448 Territorial Hwy.

Tel. Riser
 19-05-36-00-00501
 King Estate Winery Limited Partnership 99%
 Site Addr: 80854 Territorial Hwy.

LEGEND
 Cut Slope
 - - - - - Fill Slope
 ##### Removal of Extg. Pipe



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer
 GRAPHIC SCALE
 0 50 100
 (in Feet)

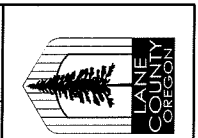
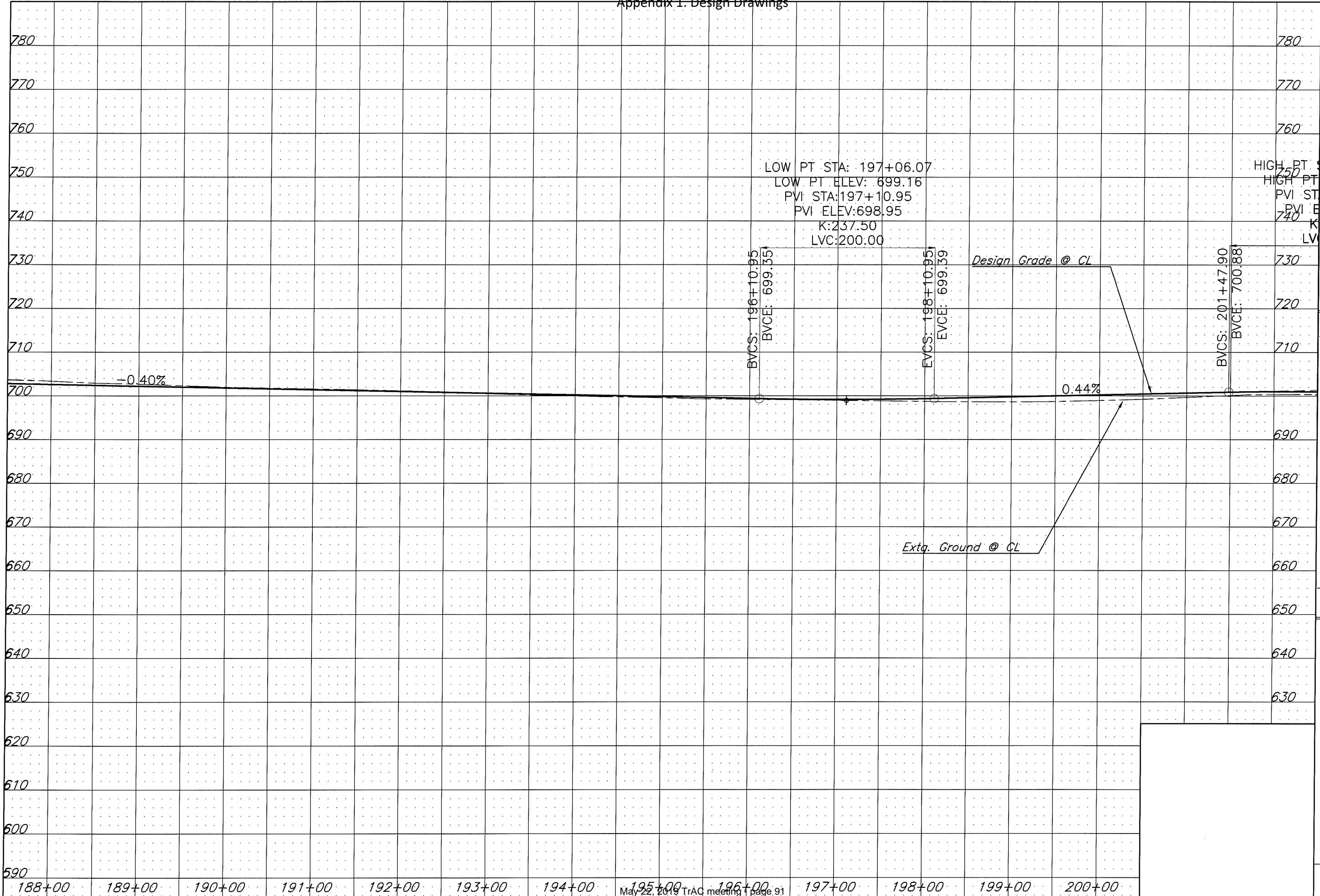
Date	Revision	Appr'd

Filename: H:\Projects\17989-02\DES\Planset\Plan&Profile\Topog.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

Appendix 1. Design Drawings

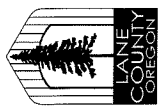


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

Filename: H:\Projects\A7989-02\DES\PlanSet\Plan&Profile_Topog.dwg



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARESHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

① Sta. L 207+99.56, 47.0' Lt.
Sawcut And Match Extg Pavement

Date	Revision	Appr'd

Filename: H:\Projects\#7889-02\DES\Planaset\Plan&Profile\CNote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

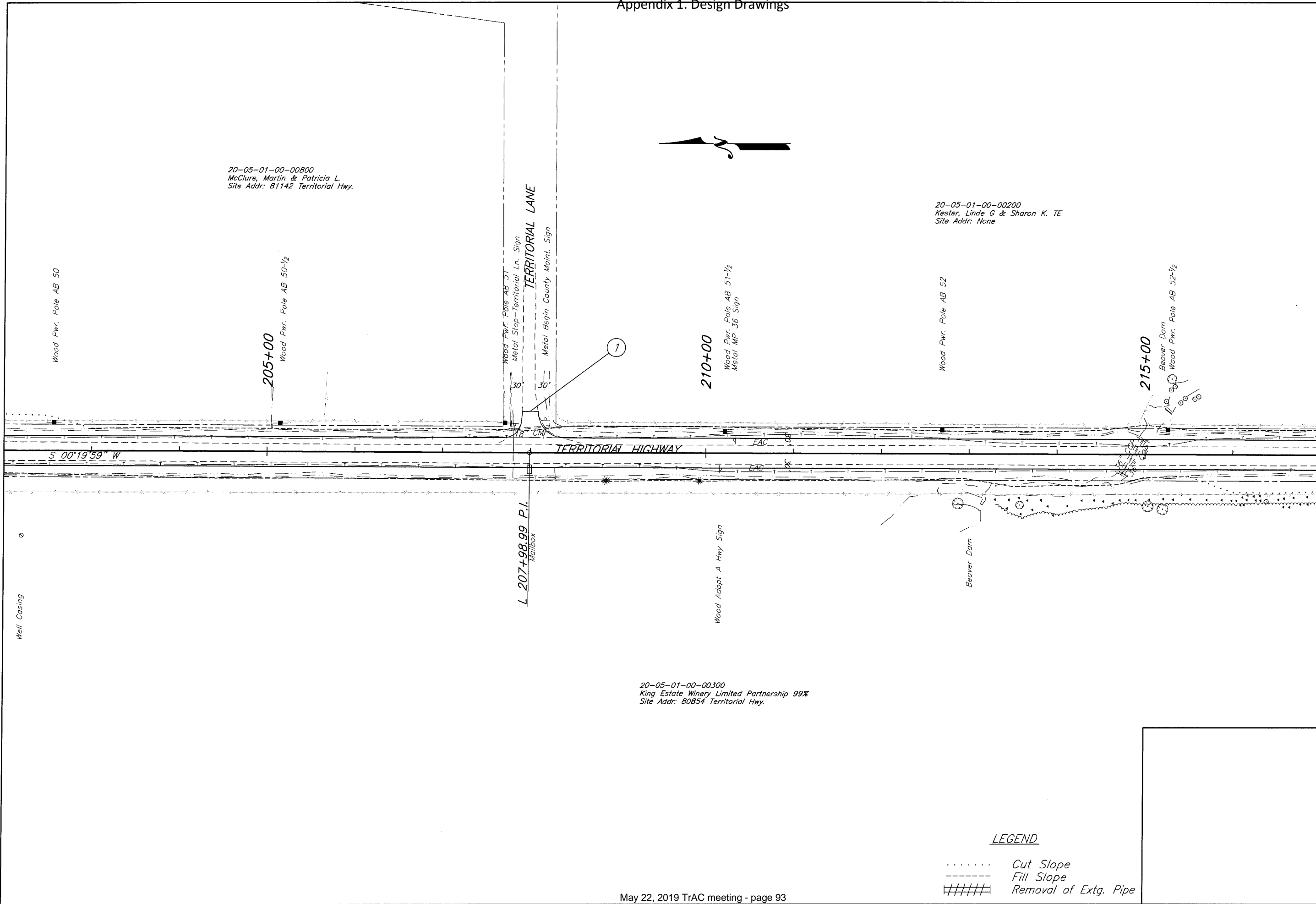
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 17

Legend

..... Cut Catch Line

----- Fill Catch Line



LEGEND
 Cut Slope
 - - - - - Fill Slope
 ##### Removal of Extg. Pipe

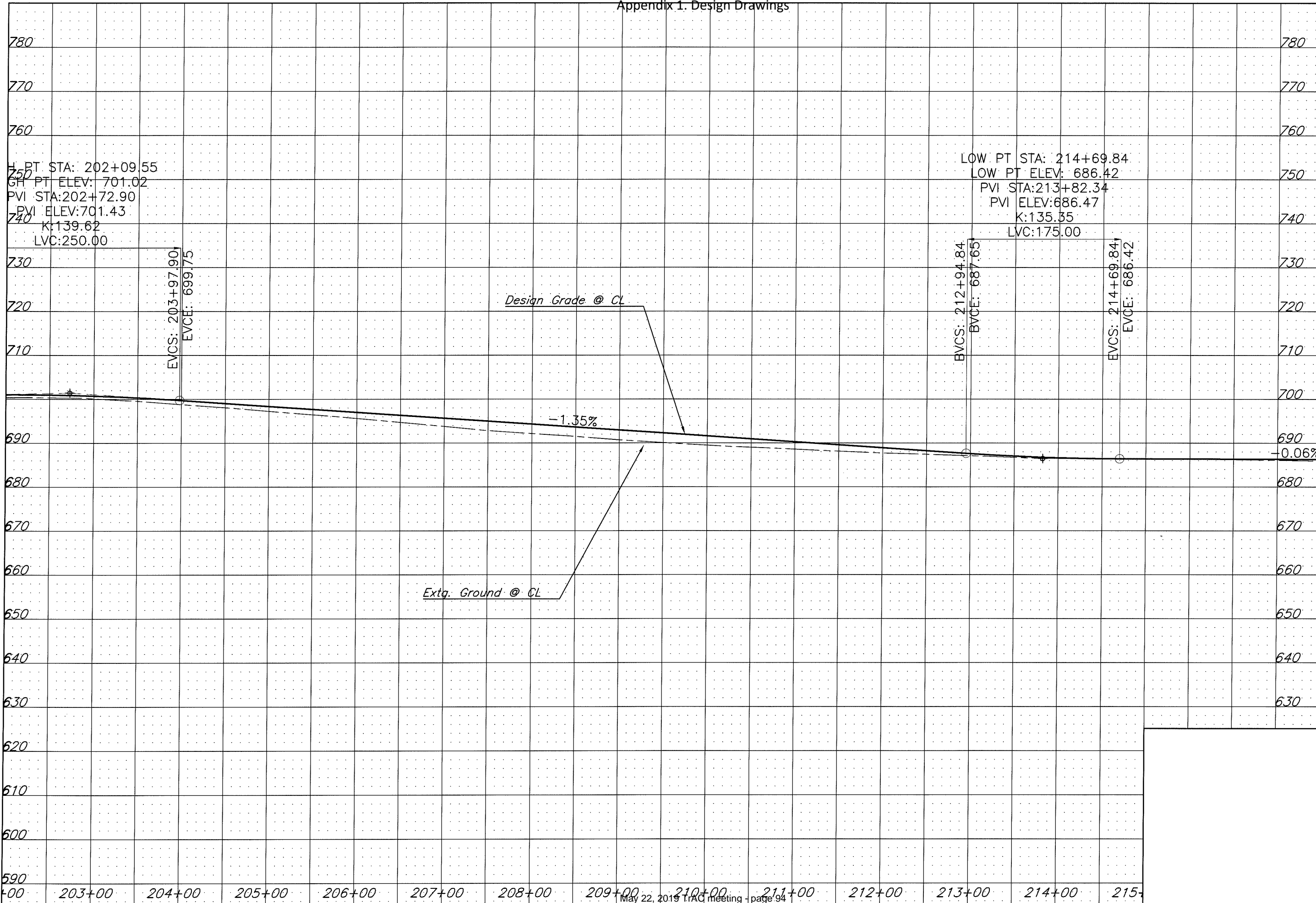


LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer
 GRAPHIC SCALE
 0 50 100
 (In Feet)

Date	Revision	Appr'd

Filename: H:\Projects\147989-02_DES\Pinest\Plan&Profile\1909.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)
PROJECT NO.	XXXXXX



LANE COUNTY
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ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY
DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXX



LANE COUNTY
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 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
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 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

Filename: H:\Projects\#17989-02\DES\PlanSet\Plan&Profile\Notes.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

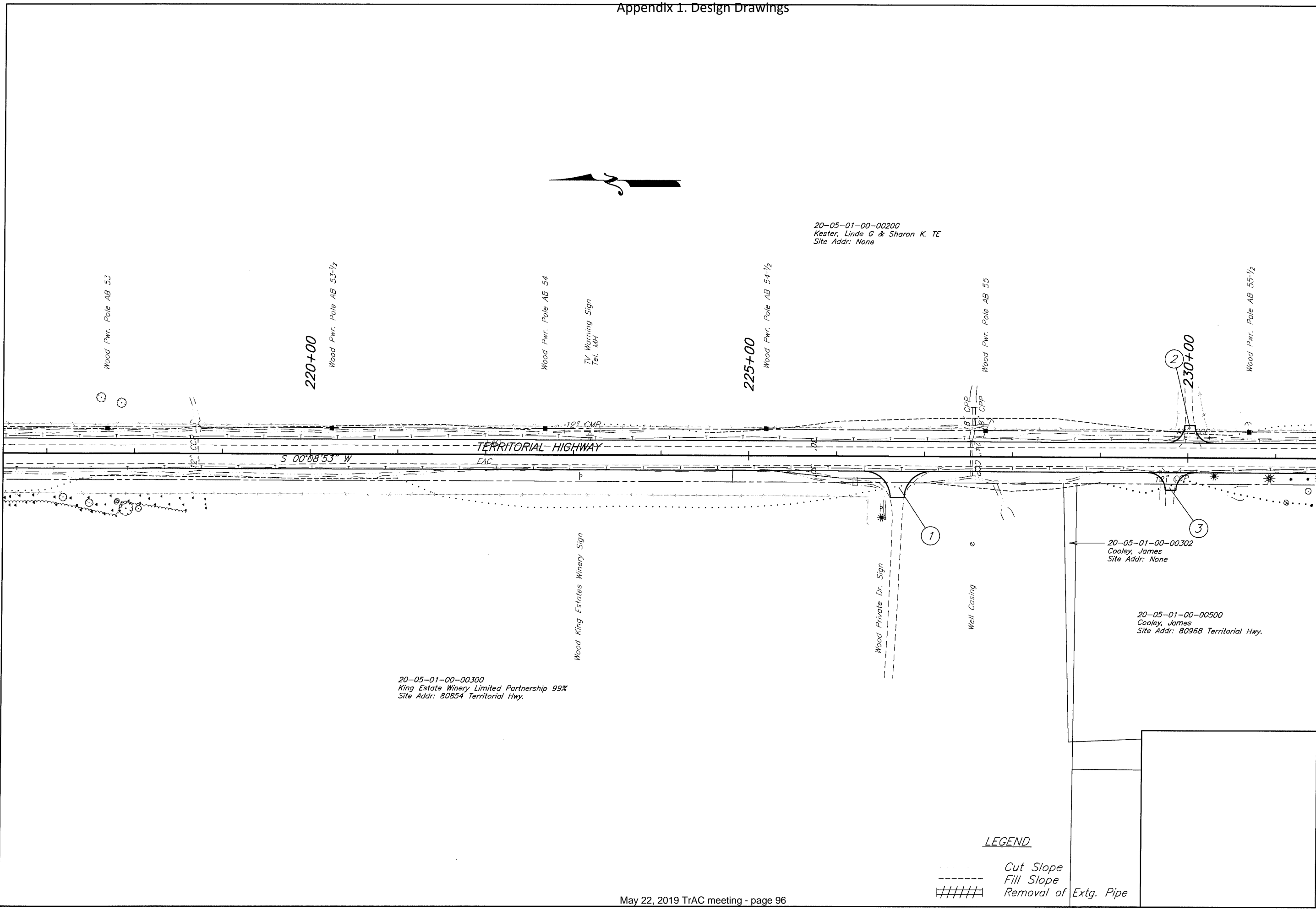
SHEET NO. 18

- ① Sta. L 226+69.54 Rt.
Const. Asph. Conc. Drwy. - 16' Wide
(For Details, See Dwg. RD715)
- ② Sta. L 230+01.94 Lt.
Const. Asph. Conc. Drwy. - 12' Wide
- ③ Sta. L 229+80.52 Rt.
Const. Asph. Conc. Drwy. - 12' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line



LANE COUNTY
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 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

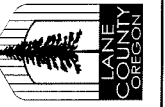
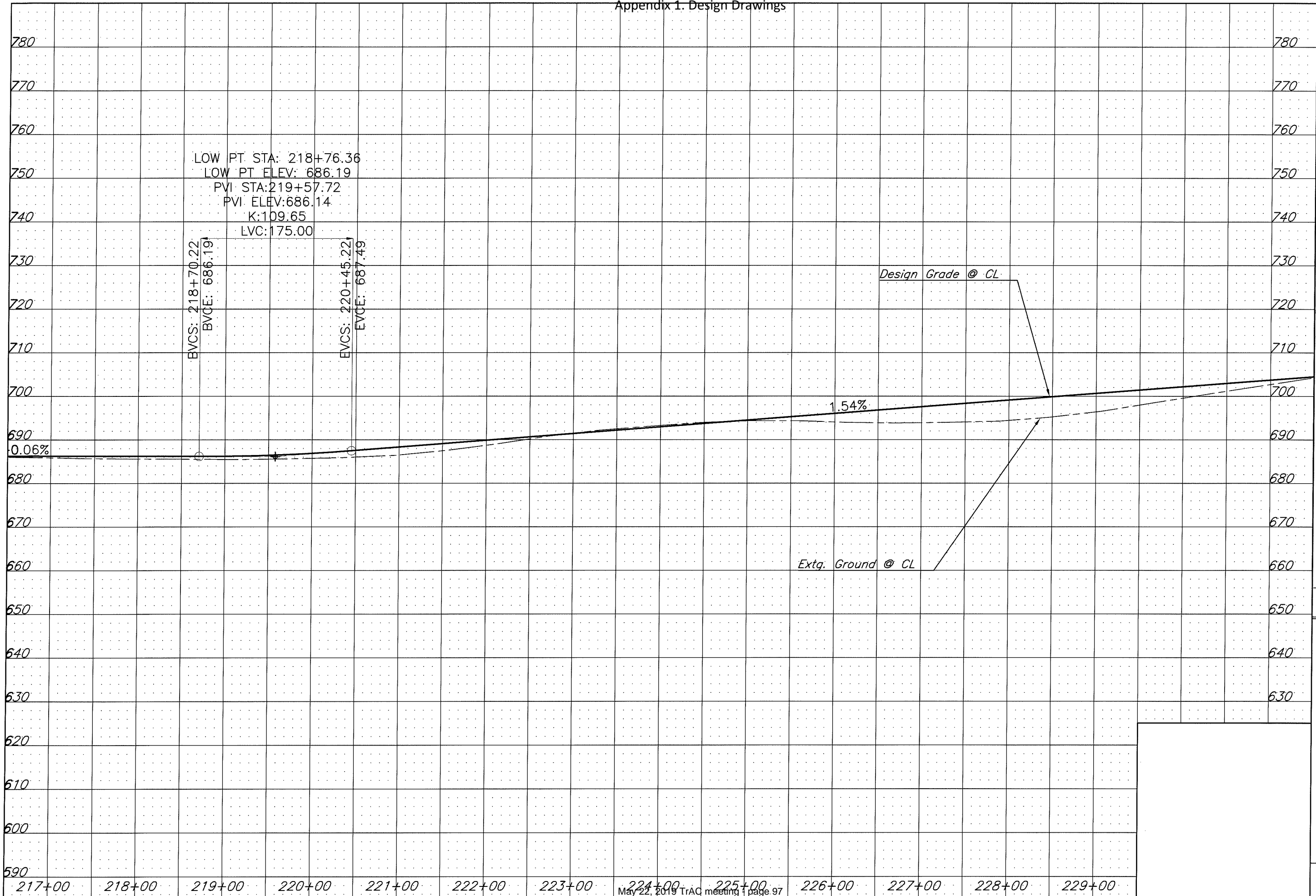


Revision	Date	Appr'd

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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
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PROJECT NO.	
STP-XXXX(XXX)	

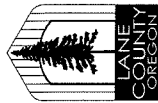
SHEET NO. 18A



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 WILLIAM F. MORGAN, P.E.
 County Engineer
 MARSHA A. MILLER
 Public Works Director

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY
 DATE: XXXXXX 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX



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 WILLIAM F. MORGAN, P.E.
 County Engineer
 MARSHA A. MILLER
 Public Works Director

Date	Revision	Appr'd

Filename: H:\Projects\#A7989-02\BES\Planest\Plan&Profile\Gnote.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

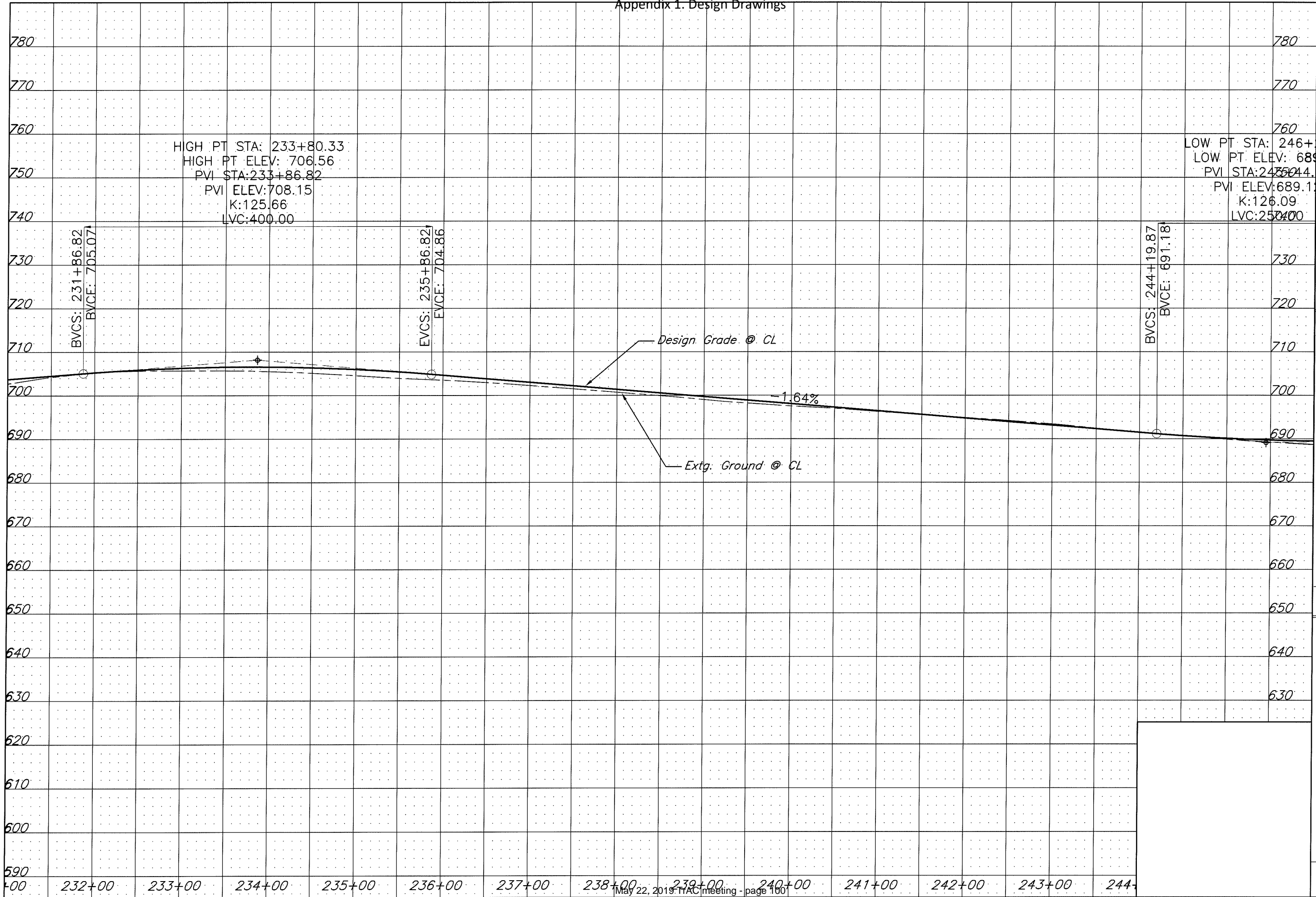
DATE: 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

SHEET NO. 19

- ① Sta. L 232+53.27 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
(For Details, See Dwg. RD715)
- ② Sta. L 235+25.40 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ③ Sta. L 237+89.60 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ④ Sta. L 238+30.84 Lt.
Const. Asph. Conc. Drwy. - 12' Wide
- ⑤ Sta. L 241+79.25 Rt.
Const. Asph. Conc. Drwy. - 38' Wide
- ⑥ Sta. L 242+83.20 Rt.
Const. Asph. Conc. Drwy. - 19' Wide
- ⑦ Sta. L 240+49.32 Lt.
Const. Asph. Conc. Drwy. - 12' Wide
- ⑧ Sta. L 245+18.19 Rt.
Const. Asph. Conc. Drwy. - 21' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line

Appendix 1. Design Drawings



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
 County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARESHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer

Date	Revision	Appr'd

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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE: 201X
 PROJECT NO.: STP-XXXX(XXX)
 STATE HWY. NO.: XXXXXX

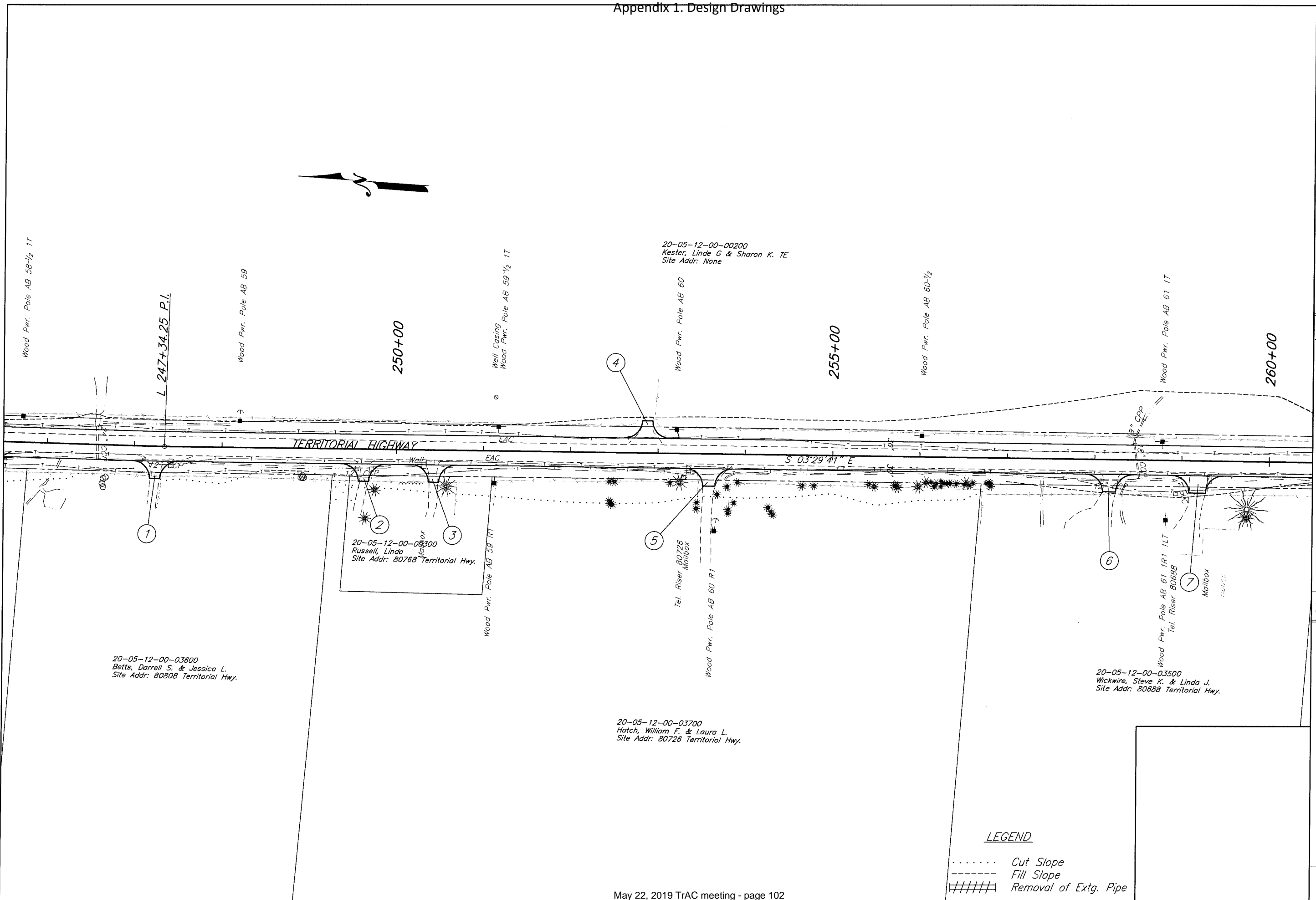
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- ① Sta. L 247+24.38 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
(For Details, See Dwg. RD715)
- ② Sta. L 249+63.15 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ③ Sta. L 250+43.23 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ④ Sta. L 252+82.29 Lt.
Const. Asph. Conc. Drwy. - 12' Wide
- ⑤ Sta. L 253+58.87 Rt.
Const. Asph. Conc. Drwy. - 14' Wide
- ⑥ Sta. L 258+17.11 Rt.
Const. Asph. Conc. Drwy. - 19' Wide
- ⑦ Sta. L 259+18.04 Rt.
Const. Asph. Conc. Drwy. - 19' Wide

Legend
 Cut Catch Line
 ----- Fill Catch Line



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

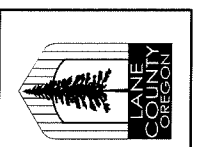
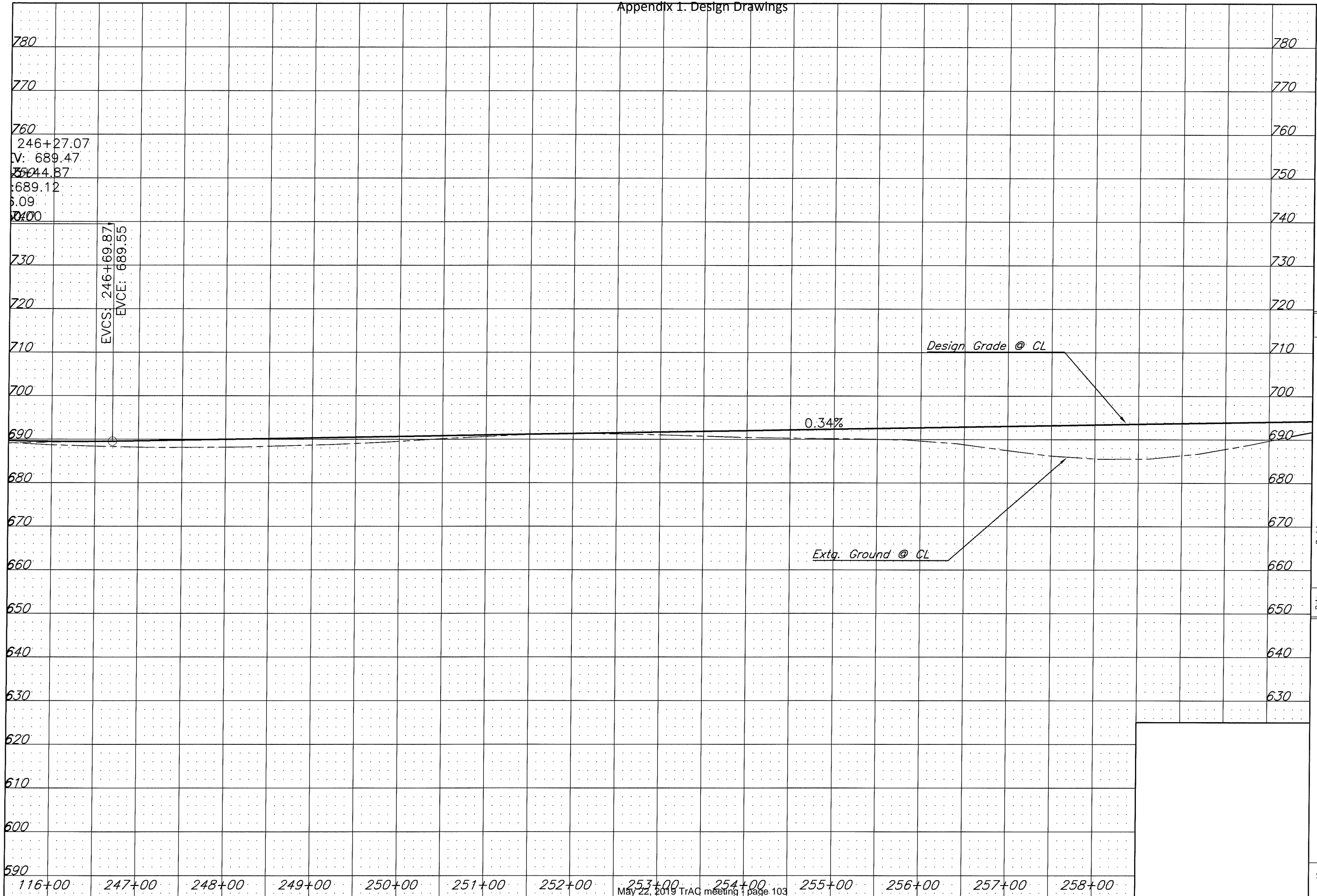


Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	XXXXXX 201X	DATE	XXXXXX

SHEET NO. 20A

Appendix 1. Design Drawings



LANE COUNTY
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ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Date	Revision	App'd

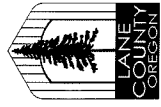
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 20B

116+00 247+00 248+00 249+00 250+00 251+00 252+00 253+00 254+00 255+00 256+00 257+00 258+00

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LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director

WILLIAM F. MORGAN, P.E.
 County Engineer

Appr'd	Revision	Date

Filename: H:\Projects\#A7989-02\DES\Planset\Plan&Profile\Profile\Note.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

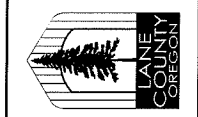
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- ① Sta. L 263+78.47 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
(For Details, See Dwg. RD715)
- ② Sta. L 264+52.57 Rt.
Const. Asph. Conc. Drwy. - 12' Wide
- ③ Sta. L 268+61.26, 63.4' Rt.
Sawcut And Match Extg Pavement
- ④ Sta. L 269+33.86, 56.6' Rt.
Sawcut And Match Extg Pavement
- ⑤ Sta. L 270+83.11 Rt.
Const. Asph. Conc. Drwy. - 25' Wide
- ⑥ Sta. L 271+17.67 Lt.
Const. Asph. Conc. Drwy. - 14' Wide
- ⑦ Sta. L 266+77.76 Rt.
Const. Asph. Conc. Drwy. - 12' Wide

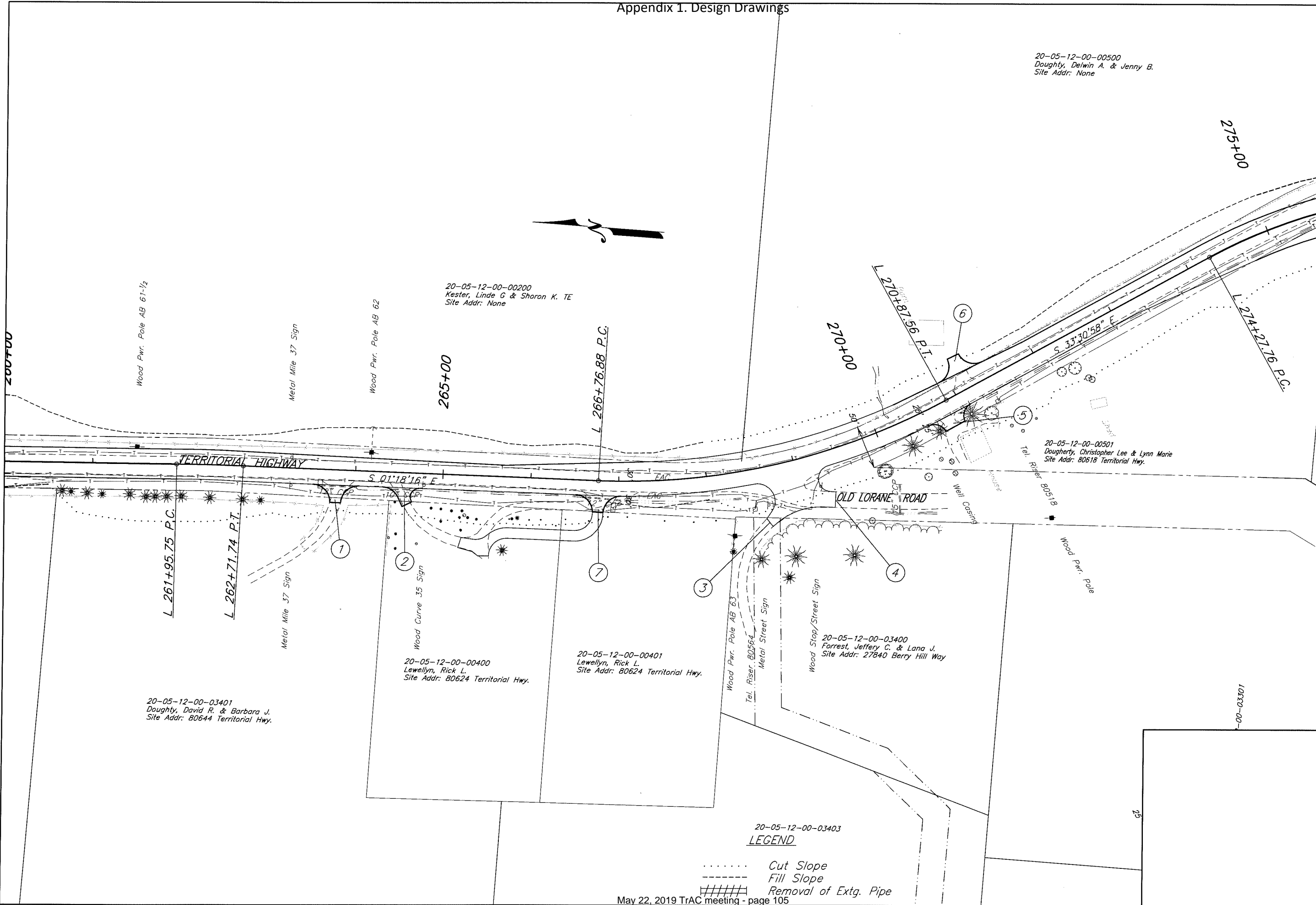
Legend

- Cut Catch Line
- Fill Catch Line

20-05-12-00-00500
Doughty, Delwin A. & Jenny B.
Site Addr: None



LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN P.E.
County Engineer



20-05-12-00-00200
Kester, Linde G & Sharon K. TE
Site Addr: None

20-05-12-00-03401
Doughty, David R. & Barbara J.
Site Addr: 80644 Territorial Hwy.

20-05-12-00-00400
Lewellyn, Rick L.
Site Addr: 80624 Territorial Hwy.

20-05-12-00-00401
Lewellyn, Rick L.
Site Addr: 80624 Territorial Hwy.

20-05-12-00-03400
Forrest, Jeffery C. & Lana J.
Site Addr: 27840 Berry Hill Way

20-05-12-00-00501
Doughty, Christopher Lee & Lynn Marie
Site Addr: 80618 Territorial Hwy.

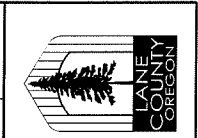
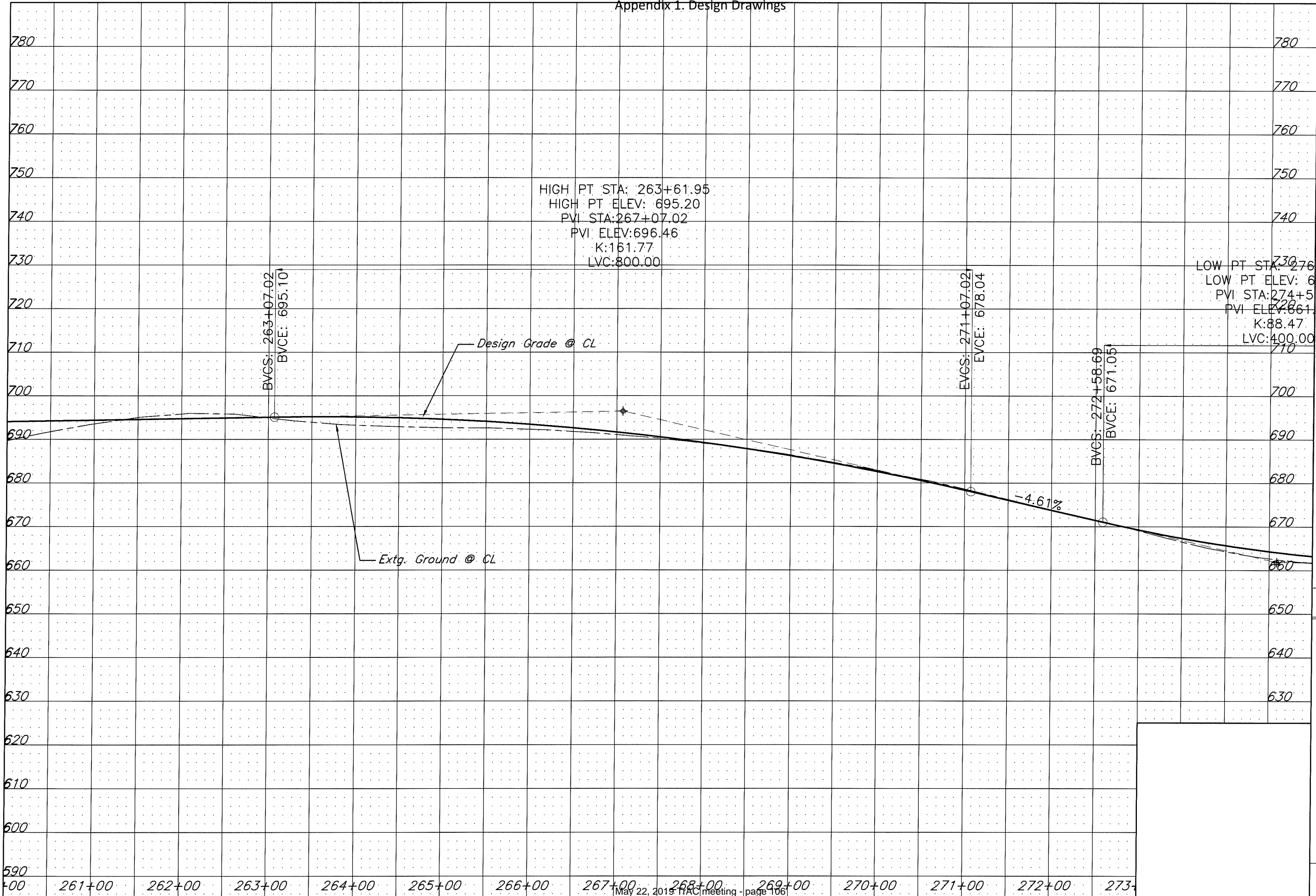
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- Cut Slope
- - - - - Fill Slope
- ##### Removal of Extg. Pipe

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	201X	DATE	XXXXXX

SHEET NO. 21A



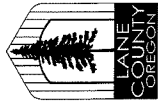
LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director

WILLIAM F. MORGAN, P.E.
County Engineer

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

DATE: XXXXXX 201X
PROJECT NO.: STP-XXXX(XXX)
STATE HWY. NO.: XXXXXX



See Adjacent Sheet

LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
WILLIAM E. MORGAN, P.E.
County Engineer
MARSHA A. MILLER
Public Works Director

Date	Revision	Appr'd

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GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY

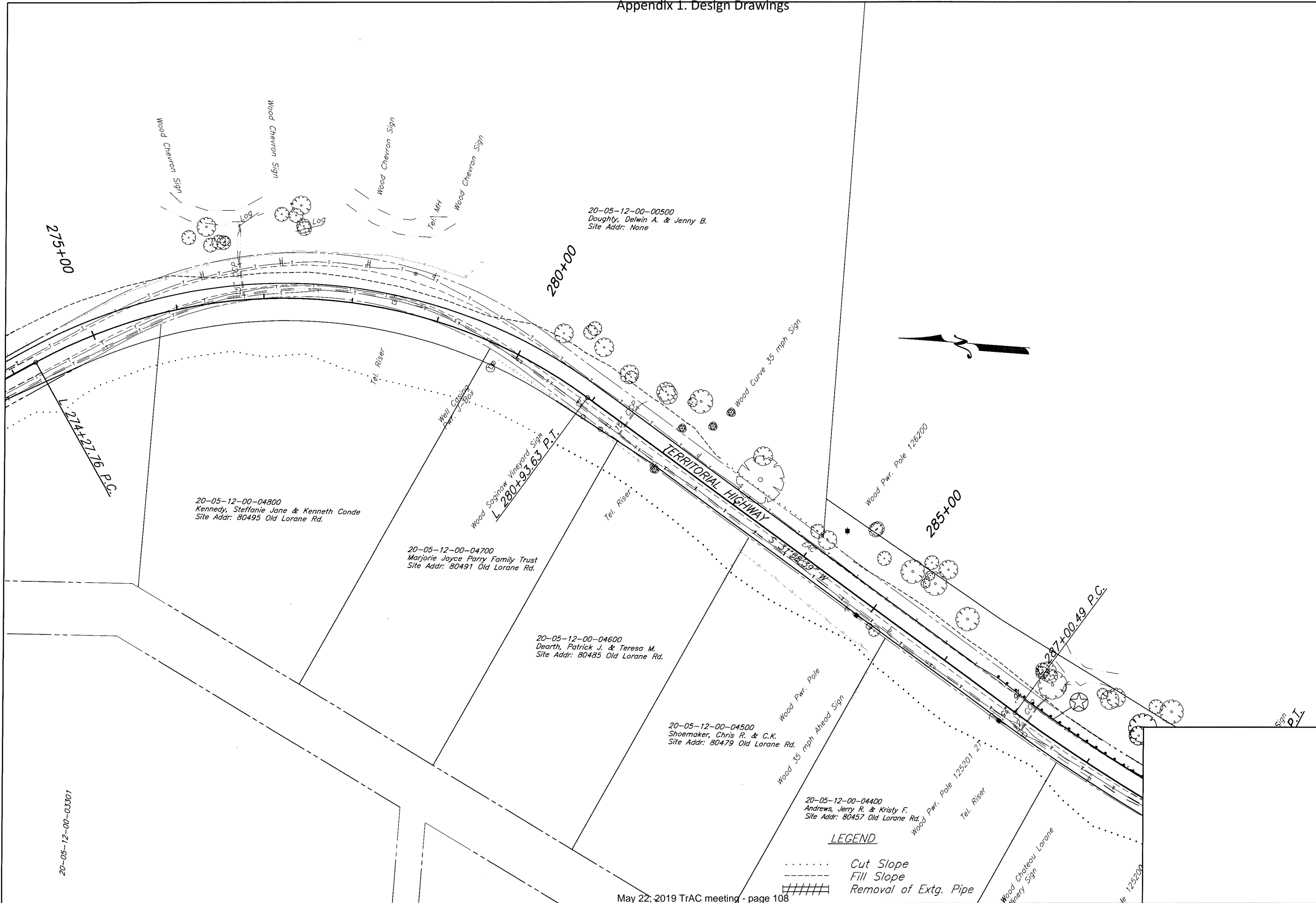
DATE	PROJECT NO.	STATE HWY. NO.
XXXXXXXX 201X	STP-XXXX(XXX)	XXXXXX

SHEET NO. 22

Legend

..... Cut Catch Line

----- Fill Catch Line



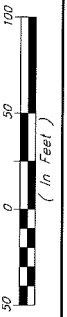
LEGEND

- Cut Slope
- Fill Slope
- ##### Removal of Extg. Pipe

May 22, 2019 TrAC meeting - page 108



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN, P.E.
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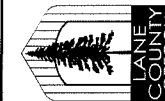


Date	Revision	Appr'd

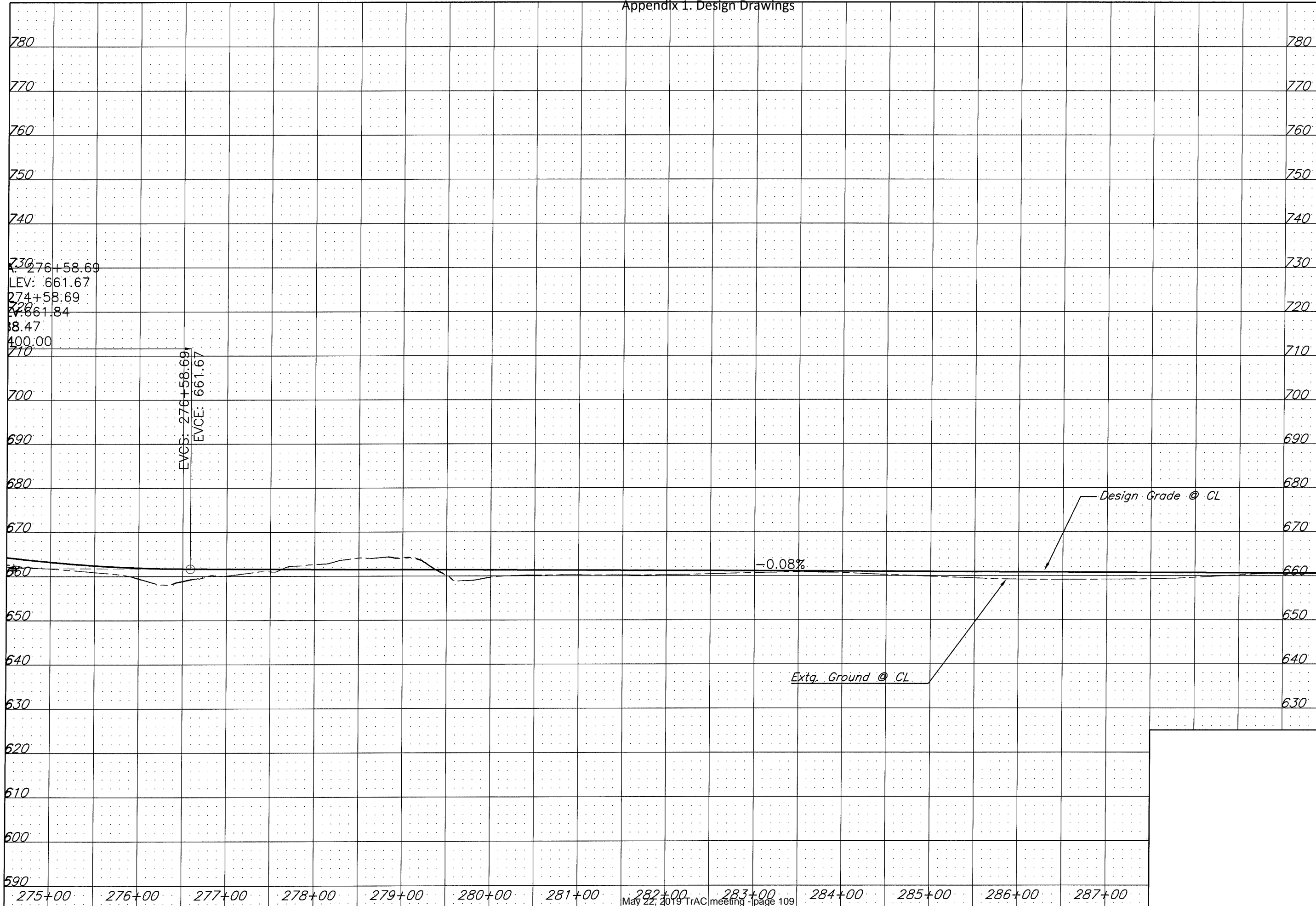
GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD		STATE HWY. NO.	XXXXXX
TERRITORIAL HIGHWAY		PROJECT NO.	STP-XXXX(XXX)
DATE	XXXXXX 201X		

SHEET NO. 22A

Filename: H:\Projects\47989-02_VES_PlanSet\Plan&Profile_Topos.dwg



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 MARSHA A. MILLER
 Public Works Director
 WILLIAM F. MORGAN P.E.
 County Engineer



Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	STATE HWY. NO.
XXXXXX 201X	STP-XXXX(XXX)
PROJECT NO.	XXXXXX
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Date	Revision	Appr'd

Filename: H:\Projects\#17989-02\DES\PlanSet\Plan&Profile\Note.dwg

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
 TERRITORIAL HIGHWAY

PROJECT NO. STP-XXXX(XXX)
 STATE HWY. NO. XXXXXX

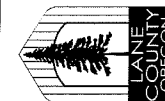
DATE
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SHEET NO. 23

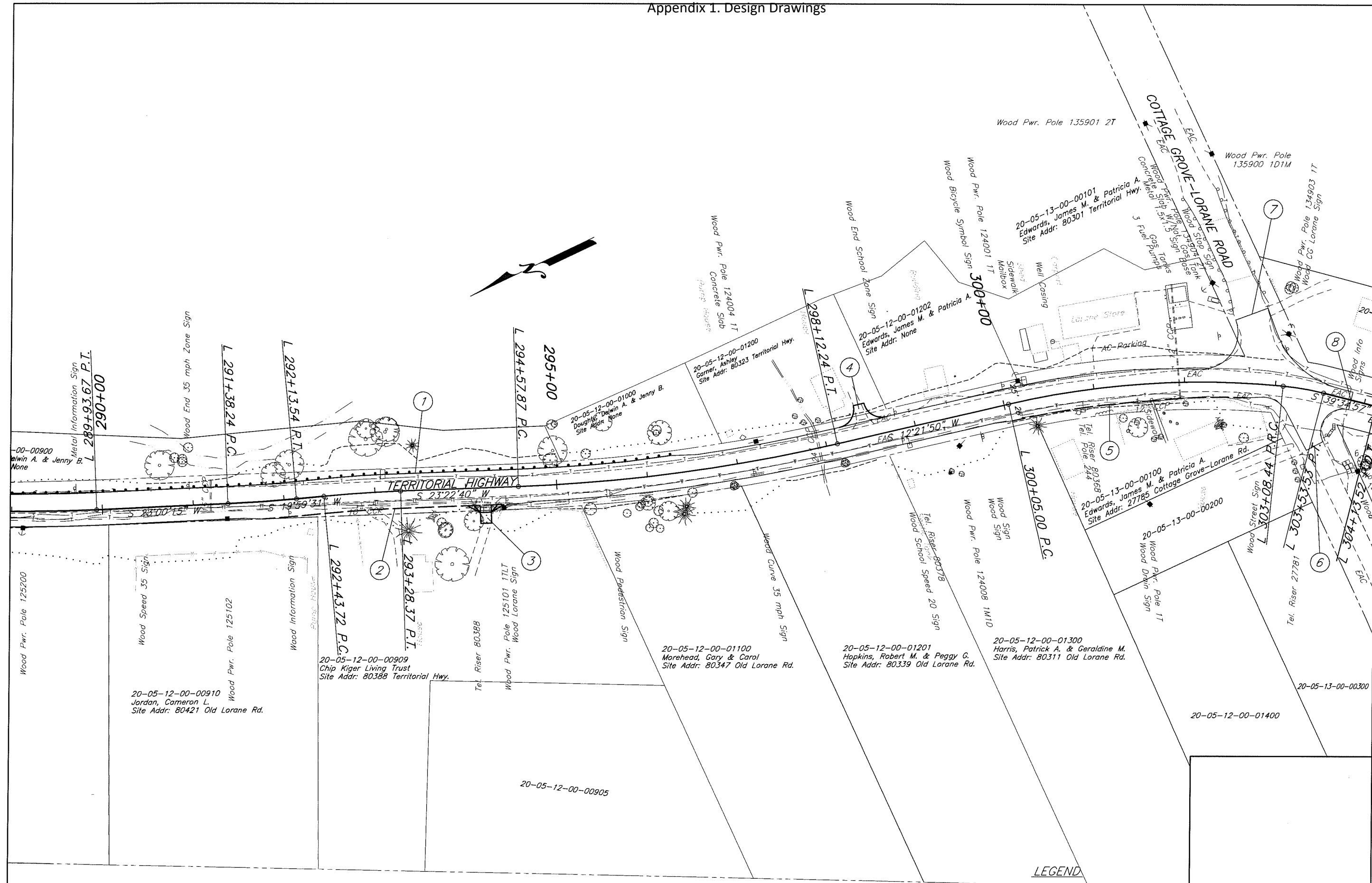
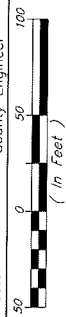
- ① Sta. L 286+60.70 Lt. To Sta. L 296+27.78 Lt.
 Const. Guardrail, Type 2A - 862.5 Ft.
 Const. Guardrail Terminal, Energy Absorbing (Non-Flared) - 2 Each
 W=1.0', E=2.0', L=50', Test Level 3
 (For Details, See Dwg. RD400, RD405, RD415 and RD420)
- ② Sta. L 291+75.27 Rt. To Sta. L 294+57.87 Rt.
 Const. Conc. Curb And Gutter
 (For Details, See Dwg. RD700)
- ③ Sta. L 294+21.40 Rt.
 Const. Conc. Drwy. - 12' Wide
 (For Details, See Dwg. RD730)
- ④ Sta. L 298+44.45 Lt.
 Const. Asph. Conc. Drwy. - 12' Wide
 (For Details, See Dwg. RD715)
- ⑤ Sta. L 300+05.48 Rt. To Sta. L 302+68.57 Rt.
 Const. Conc. Curb And Gutter
- ⑥ Sta. L 303+58.52, 58.6' Rt.
 Sawcut And Match Extg Pavement
- ⑦ Sta. L 302+69.34, 76.4' Lt.
 Sawcut And Match Extg Pavement
- ⑧ Sta. L 303+84.73
 Sawcut And Match Extg Pavement

Legend

- Cut Catch Line
- Fill Catch Line



LANE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING AND CONSTRUCTION SERVICES DIVISION
 WILLIAM F. MORGAN, P.E.
 County Engineer
 MARSHA A. MILLER
 Public Works Director



LEGEND

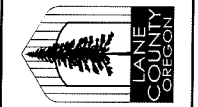
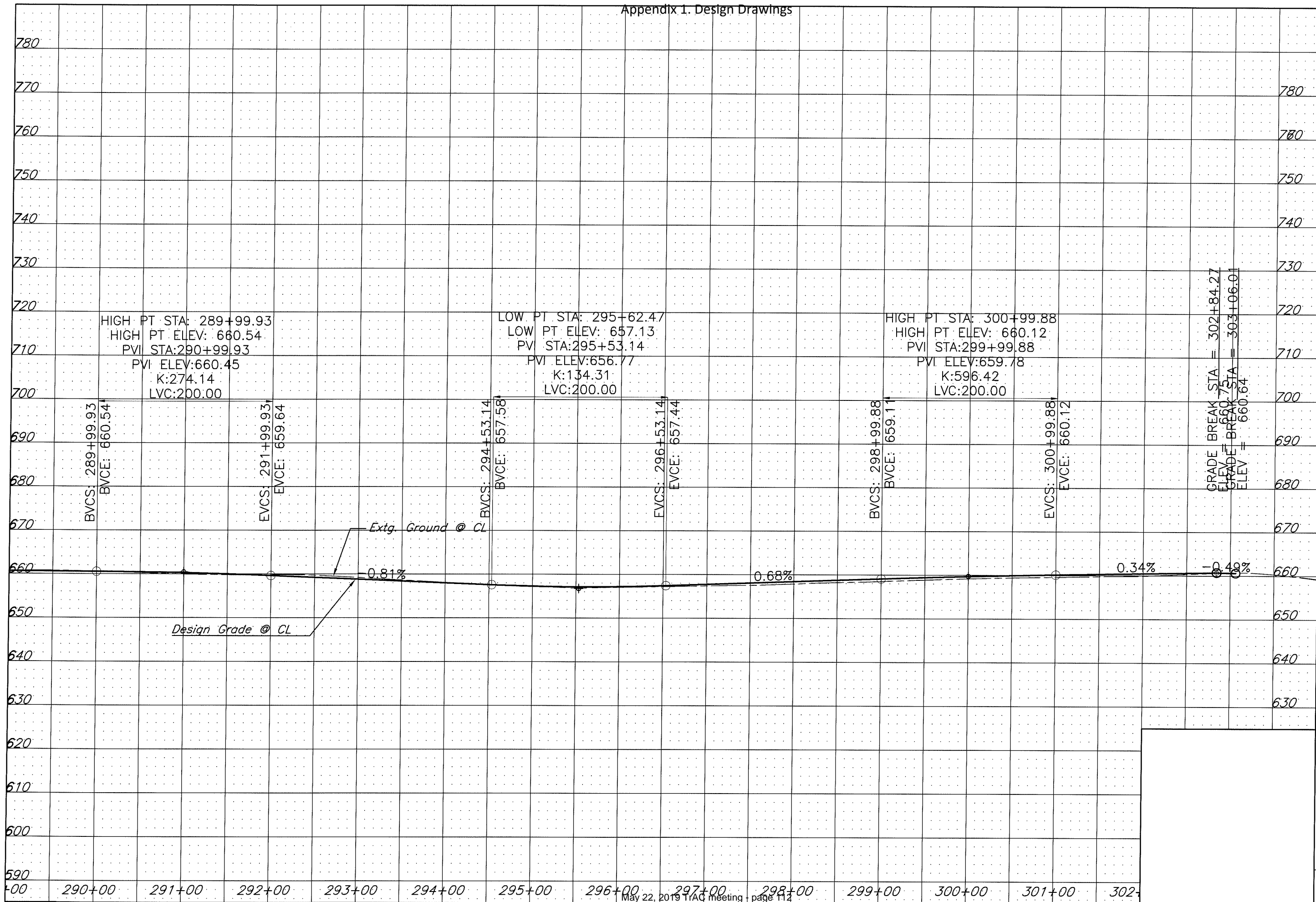
- Cut Slope
- - - - - Fill Slope
- ##### Removal of Extg. Pipe

Date	Revision	Appr'd

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD	
TERRITORIAL HIGHWAY	
DATE	PROJECT NO.
XXXXXX 201X	STP-XXXX(XXX)
STATE HWY. NO.	XXXXXX

SHEET NO. 23A

Appendix 1. Design Drawings



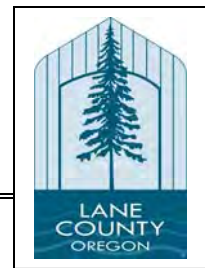
LANE COUNTY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING AND CONSTRUCTION SERVICES DIVISION
MARSHA A. MILLER
Public Works Director
WILLIAM F. MORGAN, P.E.
County Engineer

Appr'd	Revision	Date

GILLESPIE CORNERS TO COTTAGE GROVE-LORANE ROAD
TERRITORIAL HIGHWAY
DATE: XXXXXX 201X
PROJECT NO: STP-XXXX(XXX)
STATE HWY. NO: XXXXXX
XXXXXXXX

SHEET NO. 23B

Memorandum Date: April 30, 2019
Meeting Date: May 22, 2019



TO: Transportation Advisory Committee (TrAC)

DEPARTMENT: Public Works

PRESENTED BY: Steve Gallup, Transportation Engineering Services Manager
Becky Taylor, Senior Transportation Planner

AGENDA ITEM: Traffic Calming Program

I. ACTION

No formal action is requested of the TrAC. This is an informational item to prepare the TrAC for a more detailed discussion on this project at the July 24, 2019 meeting. This project is for the creation of a new program with a proposed pilot project that would likely compete with other demands of the \$500K/year fund earmarked in the Capital Improvement Program (CIP) for safety improvements.

II. BACKGROUND

Transportation Engineering Services (TES) would like to create a traffic calming program for Lane County. Traffic calming is the combination of mainly physical measures to slow speeds for motor vehicles, reduce non-local motor vehicle traffic, reduce traffic crash frequency and severity, and increase the safety and perception of safety for non-motorized users of the road. There are a wide range of physical measures (e.g. pavement markings, curb extensions, and raised crosswalks) that Lane County can consider as part of safety improvement projects which does not require the development of a traffic calming program.

Creation of a traffic calming program is specifically desired to provide a venue for the public to request changes in their neighborhoods. Within this context, the specific traffic calming measure to be considered is speed cushions which are speed humps or speed tables that include wheel cutouts to allow large vehicles, especially emergency vehicles, to pass unaffected, while reducing passenger vehicle speeds. Eligibility criteria are prescribed to ensure safety and effectiveness.



Over the past two years, TES has received citizen requests for speed cushions on residential roads. Notably, all of the requests have been from residents in the River Road – Santa Clara neighborhoods of Eugene. Eugene has a traffic calming program that enables citizen-requested speed cushions to be installed, subject to eligibility and approval criteria. Currently, Lane County is unable to consider similar requests because we do not have a traffic calming

program.

TES currently responds to neighborhood speeding requests through its speed feedback sign program. TES currently has four speed feedback signs that were acquired through a safety grant and we borrow three additional signs from Maintenance when they are not in use for roadway projects. These speed feedback signs are rotated throughout Lane County, at different locations every two weeks. This program will continue and would be supplemented by the traffic calming program.

III. RECOMMENDATION / NEXT STEPS

At the July 24, 2019 TrAC meeting, staff will share the proposed traffic calming process, such as application screening and eligibility criteria. TrAC input is needed on the application process and approval criteria which would include funding consideration. Staff will also share information about recent citizen requests for speed cushions and roadway data being used to shape the new program and identify the best candidate for a pilot project.

IV. FOR MORE INFORMATION

Feel free to contact Becky Taylor by phone at 541-682-6932 or by email at becky.taylor@co.lane.or.us.

V. ATTACHMENTS

No attachments.



Lane County Transportation Advisory Committee (TrAC)

Tentative 12-Month Calendar & Agenda Items

<p style="text-align: center;">January 23, 2019</p> <ul style="list-style-type: none"> • Nominations / Appointments: 2019 Chair / Vice Chair LaneACT representative • LaneACT presentation from Frannie Brindle • Begin CIP project prioritization discussion • 2018 Year-End Report / 2019 Next Steps 	<p style="text-align: center;">March 27, 2019</p> <ul style="list-style-type: none"> • Compile and review list of CIP priority projects • Transportation Safety Implementation Update • Road Maintenance Update 	<p style="text-align: center;">May 22, 2019</p> <ul style="list-style-type: none"> • Refinement of CIP priority projects as needed • Local Access Roads policy discussion • Project Updates: Territorial Hwy
<p style="text-align: center;">July 24, 2019</p> <ul style="list-style-type: none"> • Review draft of CH 15 updates • Refinement of CIP list • Territorial Highway Design: Public Hearing: Review and make recommendation to Board • Traffic Calming proposal • Local Access Roads next steps 	<p style="text-align: center;">September 25, 2019</p> <ul style="list-style-type: none"> • Public Hearing: FY20-FY25 Capital Improvement Program 	<p style="text-align: center;">October 2019 (Final Date to be determined)</p>
<p style="text-align: center;">November 27, 2019</p>	<p style="text-align: center;">January 22, 2020</p>	<p style="text-align: center;">March 25, 2020</p>